

THE WHEAT QUARTER

DESIGN & ACCESS STATEMENT




DESIGN & ACCESS STATEMENT

NOVEMBER 2020

ColladoCollins Architects

THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT - NOVEMBER

PROJECT TEAM

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1. INTRODUCTION

THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

1.1. PURPOSE OF THIS DOCUMENT

ColladoCollins Architects have been appointed by Wheat Quarter Limited to prepare this Design and Access Statement, which accompanies the planning application for the redevelopment of the land at Broadwater Road, known locally as the former Shredded Wheat Factory site.

The purpose of this Design and Access Statement is to provide an overview of the design proposal for the former Shredded Wheat Factory site. The document reviews several aspects of the relevant context (location, planning guidance, precedent designs); it summarises the collaborative process which lead to the design proposal; and it discusses specific places and buildings within the application site, offering some details and principles for further design work. More specifically this document sets out where changes have been made to the layout and design when compared to the extant consent.

The legacy of Welwyn Garden City as a planned environment and the prominent, gateway location of the Shredded Wheat factory site within it accentuates the need for a striking, landmark development for the town. Having been informed by a thorough consultation process and design legacy, this proposal is the second step in an effort to establish a new, thriving community within Welwyn Garden City; to enrich the town with new, active and inviting places; and to lay out an inspiring vision for the future of the Wheat Quarter site, at the turn of WGC's first century.

1.2. DESCRIPTION OF THE DEVELOPMENT

Hybrid planning application comprising: Detailed Planning Application for circa 399 Class C3 Private Rented Sector (PRS) dwellings, circa 250 unit Class C2 residential care community, circa 153 Class C3 dwellings, a community and commercial hub comprising Use Classes E and F1 floorspace and a Cycling Hub with associated car parking, access, landscaping, public art and other supporting infrastructure; Outline Planning Application for circa 418 Class C3 dwellings.

1.3. INTRODUCTION AND BACKGROUND

The Former Shredded Wheat Factory site is a unique and vital major development opportunity within Welwyn Hatfield District. It has the potential to deliver transformational development and create a new civic quarter and a thriving, sustainable community at the heart of the town.

The Wheat Quarter is a key gateway into Welwyn Garden City town centre from the east and a key gateway into the eastern part of the town from the Railway Station. It is a high profile site for a host of reasons:- its physical prominence and location, its size and its potential to accommodate a very significant amount of development, its longstanding vacancy and obvious suitability for a comprehensive development, and its heritage, given the Shredded Wheat factory was so synonymous with the Garden City.

The Former Shredded Wheat Factory site is in two ownerships, with the southern half of the Wheat Quarter site owned by Metropolitan Thames Valley Housing Association (MTVH) and the northern half of the Wheat Quarter site owned by The Wheat Quarter Ltd. The ownership position is reflected in the latest planning permission, which whilst being a single planning permission spanning both ownerships, has a suite of planning conditions and clauses cognisant of the dual ownership to enable the Wheat Quarter sites to be developed independently of one another. This document has been prepared for The Wheat Quarter Ltd and relates solely to the northern part of the Wheat Quarter site. However, the scheme has been considered and designed with regard to the changes being proposed on the MTVH Site by MTVH.

Our vision for the Wheat Quarter can be summarised as follows:

- A new cultural and commercial hub within the listed former Shredded Wheat Factory and silos.
- A larger variety of homes
- Maximisation of Welwyn Garden City's most sustainably located site
- Improved connection to the Towncentre;

We think our vision resonates with the ambitions of Welwyn Hatfield for its residents, and we think this site presents an especially sustainable opportunity to deliver this vision and those ambitions. Not least, it is considered that this site represents a significant opportunity to demonstrably boost the supply of housing to address identified need in accordance with planning policy of all levels.

Alongside our vision for the Wheat Quarter site and our strategy for accommodating homes for a wider range of households and household incomes, at the First Pre-Application Meeting we presented initial development parameters which we felt could achieve these ambitions.

Since that meeting the project team has worked to refine that layout and massing in order to arrive at an acceptable layout and massing proposal. This period of refinement has included assessment based on the following ambitions:

- Appropriately locating the buildings on site in accordance with the 2019 Planning Permission.
- Ensuring a good level of daylight, sunlight, and vertical sky component to homes and open space.
- Providing an appropriate quality and quantity of amenity space, including space which is publicly, communally and privately accessible.
- Maximising current and future connectivity to surrounding residential neighbourhoods.
- Preserving the pedestrian gateway into the Wheat Quarter site along Hydeway, on approach from the east and west.
- Preserving the vehicular gateway into the town centre at the junction of Bridge and Broadwater Road.
- Enhancing identified key views of the retained listed buildings.
- Respecting the setting of the original 1920 silos - including working with the Local Planning Authority and Historic England to ensure an appropriate relationship with the proposed buildings.
- Introducing new streets to the proposal to the west of the Listed Buildings in order to enhance and frame views of these.

Further details of the layout and massing proposals, and the refinement work behind those are provided within the body of this pack.



Wheat Quarter site boundary

1.4. PLANNING HISTORY AND DEVELOPMENT PLAN

There is an existing planning permission to redevelop the former Shredded Wheat site for 1,454 homes in a mix of tenures and a range of non-residential uses that collectively form a new civic centre underpinned by the renovation of the Grade II Listed Production Hall and Silos in the centre of the Wheat Quarter site.

The planning permission was granted in February 2019 (ref: 6/2018/0171/MAJ - 'the 2019 Planning Permission') having been designed in response to a previous and still currently extant planning permission granted in August 2017 ('the Tesco permission') for up to 850 homes and associated commercial development. The Tesco scheme had been conceived and designed against the background of the adopted but now considerably out-of-date Welwyn Hatfield Local Plan and an associated Broadwater Road Supplementary Planning Document for the Development Plan allocation covering the Wheat Quarter site.

2017 Tesco Permission



The 2019 permission scheme retained many of the key design principles that underpinned the Tesco scheme (and had been first presented in the SPD) but the scheme was significantly re-worked against the backdrop of the emerging Development Plan to achieve the following:

- a considerable uplift on both market and affordable homes, from 50 to 414 affordable homes and from 850 to 1454 homes in total;
- a significant reduction in family homes and a reorientation to a scheme focused at young professionals and downsizers;

- an improved concept for the Civic Quarter, with a particular focus on international art, culture and place-making;
- a reduced amount of car parking to promote more sustainable living and working patterns;
- a broadly commensurate level of open space but with an improved network of different types of space to improve environmental quality and signpost the heritage of the Wheat Quarter site; and
- a significant increase in development contributions, including a substantial increase in education contribution to facilitate the redevelopment of Peartree School

2019 Wheat Quarter Permission



1.5. RETAINING A SHARED VISION FOR THE WHEAT QUARTER SITE

Whilst both The Wheat Quarter Ltd and MTVH are advancing separate new but complementary planning applications for the two parts of the Wheat Quarter site; retaining a shared vision for the Wheat Quarter Site and the MTVH Site, the development vision remains as per the previous scheme, which is to deliver a transformational development that secures substantial economic, social and environmental benefits for Welwyn Garden City by creating a new Civic Quarter in the heart of the town, and a vital and sustainable place, underpinned by the renovation of the Grade II Listed Production Hall and Silos.

A partnership exists between The Wheat Quarter Ltd and MTVH that has at its premise a development concept that the MTVH Site will deliver a scheme laden with significant quantum of affordable housing to maximise social regeneration, whereas the Wheat Quarter's scheme will secure a high quality comprehensive renovation of the Listed Buildings to unlock the heritage benefits of transforming such an important fallen asset into a signature 21st Century building of high social, economic and community value. The ability to achieve these two underpinning high-cost objectives is dependent on realising sufficient development value through high quality residential development to be delivered in tandem with these elements. This concept remains at the heart of the new development proposals for the former Shredded Wheat site.





1.6. UNDERSTANDING THE NORTH SITE AND OPPORTUNITIES TO FURTHER IMPROVE THE SCHEME

The character of the Wheat Quarter site was and still is defined by the post-industrial nature of the former Shredded Wheat Factory, with the retained Listed Buildings being the sole surviving structures remaining on an otherwise cleared site that is awaiting development. The full scale and magnitude of the restoration project can be best appreciated by inspecting the building at this time. It is in a sad and desperate state and requires immediate substantial investment.

Both the Tesco and 2019 planning permissions put the Listed Buildings at the very heart of the scheme.

Whilst substantial development is proposed in all directions around the buildings, the proposed urban form and layout respects the setting of the buildings/structures, with the layout, public realm, landscape and proposed land uses all aligning to put the Listed Buildings at the epicentre of the new Civic Quarter, the primary focal point for activity within the completed development and within a new environment that will enable residents and visitors to appreciate the beauty and value of the transformed building in the round.

The design concept provides legible spaces and enhances the connectivity throughout the Wheat Quarter site, while supporting a sense of community. The grid arrangement block forms on the Wheat Quarter site consist of three types: courtyard blocks, and L-shaped blocks. Each of these forms is deployed in response to specific conditions on each location – such as enforcing a hierarchy of active street frontages, the amount of space available, the arrangement of public spaces and landscape, distant views to landmarks on the Wheat Quarter site, etc.

A new planning application has the potential to deliver all of the benefits previously secured through the 2019 permission, with the potential to secure further benefits. Potential additional benefits are likely to include:

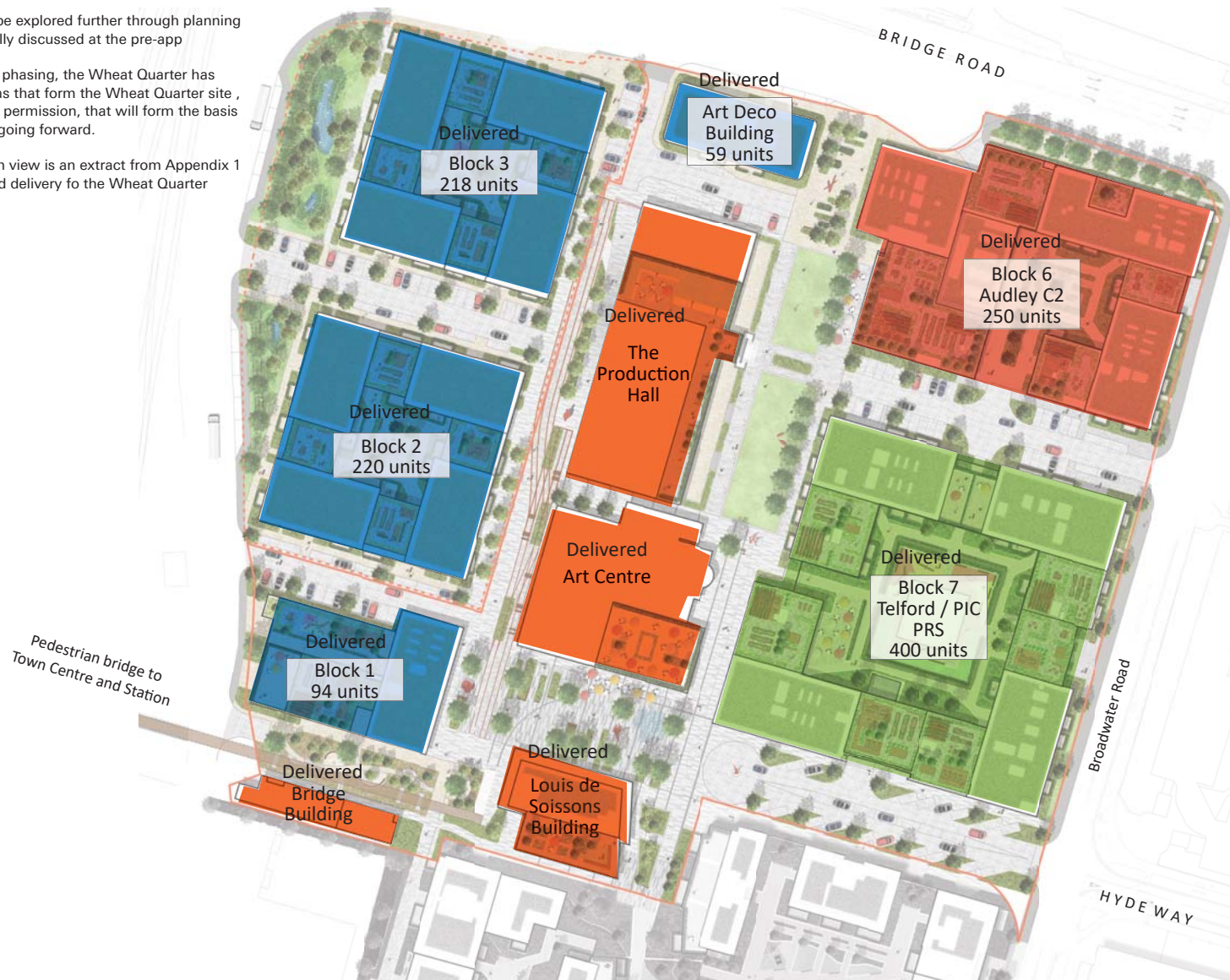
- The creation of a new and secondary pedestrian access to the west of the Wheat Quarter site, linking the development scheme to Bridge Road
- The development of a cycling strategy for the town, with the Wheat Quarter uniquely able to serve as a hub at the heart of the network to boost sustainable living and working patterns in the town
- In tandem with the above, a further reduction in car parking ratio for the Wheat Quarter site
- A redesigned western area of the Wheat Quarter site that creates more streets and more street level active frontage, responding to previous design criticisms of the podium created by the parking slab
- Increased residential development to boost housing delivery in the district, including the introduction of a purpose-designed PRS product for Welwyn Garden City

- These benefits will be explored further through planning process and hopefully discussed at the pre-app meeting.

In terms of delivery and phasing, the Wheat Quarter has identified three sub-areas that form the Wheat Quarter site, as designed in the 2019 permission, that will form the basis of the delivery strategy going forward.

The adjacent Masterplan view is an extract from Appendix 1 which covers the phased delivery to the Wheat Quarter by 2027.

COMPLETION 2027



1.7. COMPLEMENTARY DELIVERY PARTNERS

WHEAT QUARTER LTD & MAYFIELD VILLAGES
PRIVATE HOUSEBUILDER BY AUDLEY

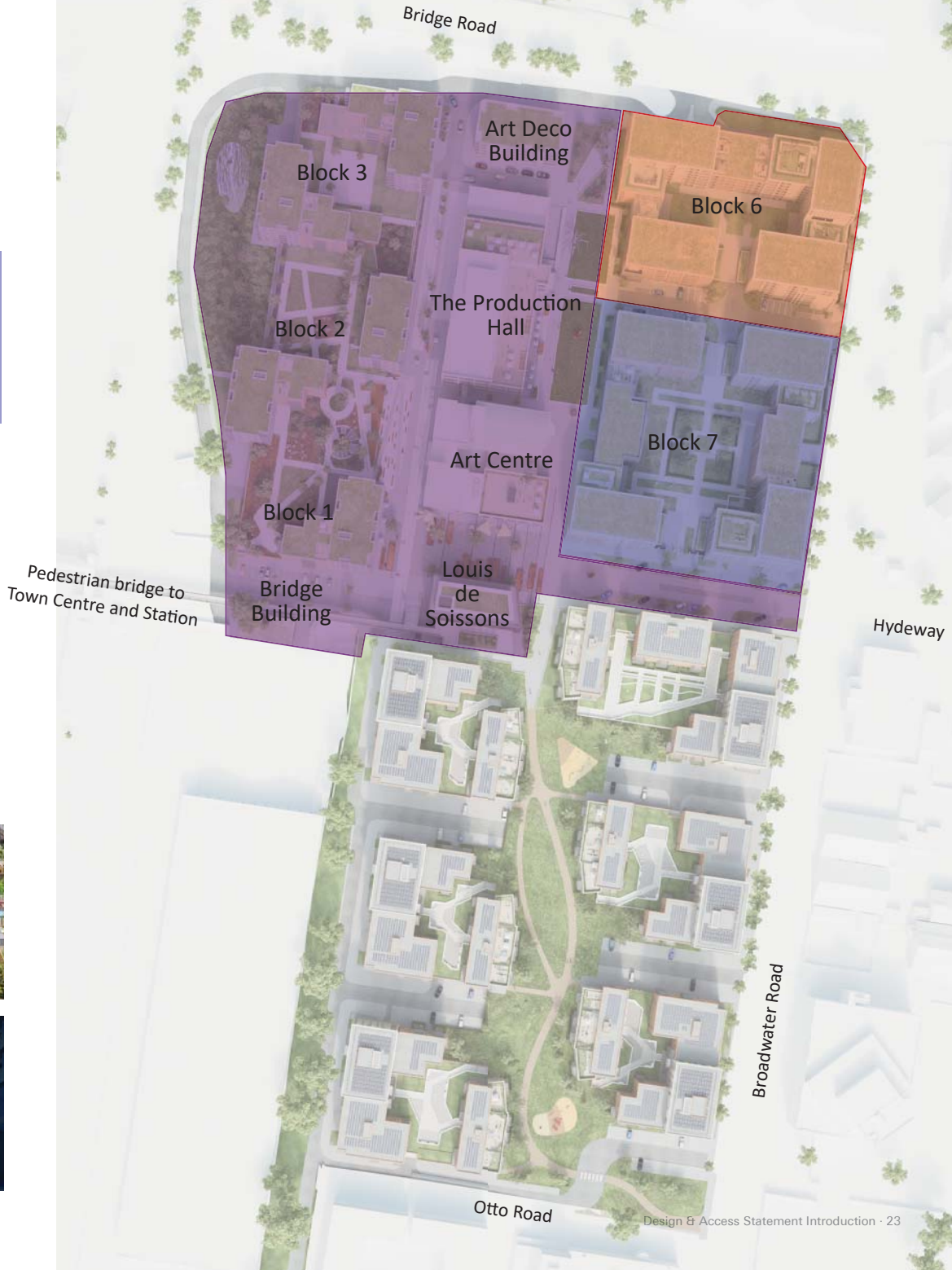
- The Production Hall
 - The Art Centre
 - Louis de Soissons Building
 - Bridge Community Building
 - Cycle Hub
 - Block 1
 - Block 2&3
 - Art Deco Building
- C.15,000sq.m of non-residential business and community floorspace including cycle hub and key public realm areas
- 153 Phase 1 residential
418 Phase 2 residential

Block 6 which is 250 units for Retirement use and full associated facilities



TELFORD HOMES & PIC PENSION FUND

Block 7 which is 399 units for PRS



1.8. RECENT HISTORY AND THE NEED FOR A NEW HYBRID PLANNING APPLICATION

After the 2019 permission was approved, The Wheat Quarter Ltd began a detailed review of the scheme focusing on delivery of the northern site, testing the strength of the housing market and looking again at the proposals and associated costs for transforming the Listed Buildings. It became apparent during this period that the housing market had weakened and was continuing to weaken, perhaps due to the threats associated with Brexit, whilst simultaneously, the true scale of the costs associated with transforming the Listed Buildings became apparent.

The Wheat Quarter held discussions with prospective housing delivery partners from all sections of the housing industry, spanning national volume housebuilders, local and regional housebuilders, specialist mixed-use developers, a variety of retirement living and care operators, as well as PRS specialists. Given the size of the Wheat Quarter site, scope exists to have different housing products being delivered across the Wheat Quarter site simultaneously to deliver the regeneration project and boost housing delivery in the Borough to help maintain a 5 year land supply.

After testing the market over this period, the Wheat Quarter has reached an agreement with a leading PRS operator to deliver circa. 400 PRS homes on Block 7 within the eastern part of the Wheat Quarter north site.

In parallel an agreement has been reached to enable a further 250 later living C2 units and ancillary amenities within Block 6, whilst an agreement for the phased redevelopment of the western side of the Wheat Quarter site has been agreed with a well known house builder with the ambition to start construction work on Block 1 which will form part of the detailed submission. A first residential phase of 802 homes could be delivered within a two year period from start of construction.

The remainder of the residential areas within the Wheat Quarter north site (Blocks 2, 3 and 6E) are likely to be brought forward as a second phase of development. Given the current state of the housing market, it is considered that delivery of the remainder of the Wheat Quarter site would ultimately be best and most quickly secured through an outline planning permission that enables developers to quickly advance Reserved Matters applications to secure a suitable housing form and layout that is compatible with their housing model. Accordingly, the area of the Wheat Quarter site currently intended for Blocks 2 and 3 would form the outline element of a hybrid planning application spanning the Wheat Quarter site.

The proposals for the Listed Buildings were reviewed and refined in 2019, with Jamie Fobert Architects appointed to review the scheme and work with Historic England to ensure the quality of the restoration and to consider the scope for amendments to secure a legacy renovation with adaptable spaces to recreate a building fit for 21st Century life.

A new planning and listed building application was submitted and subsequently approved for the amendments. Meanwhile, ColladoCollins re-worked the area to the south of the footbridge and a planning application for a Community Bridge Building was submitted but subsequently refused against the recommendation of officers. However, a planning application was approved to provide floorspace flexibility across the non-residential area and an additional floor of non-residential floorspace was secured in the Louis de Soissons Building.

All of these development agreements mean it remains the aspiration of the Wheat Quarter to maximise non-residential floorspace on the Wheat Quarter site through a new Community Bridge Building. Sadly, the application was refused, but it is hoped that some form of the building can be secured in that location of the Wheat Quarter site as part of a this hybrid application.

Detailed Planning Submission
Outline Planning Submission



1.9. DESIGN BRIEF & OBJECTIVES

The design proposal presented in this document has specifically retained the garden city principles outlined in the Broadwater Road West Supplementary Planning Document (BRW SPD), and which underpinned the design approach for the consented scheme. Building footprints and landscape design, in particular, depart as little as possible from the consented scheme.

Nevertheless, the proposed scheme differs materially to the consented scheme, most importantly by increasing the number of dwellings while also significantly increasing the diversity of the residential use types by introducing a significant increase in C2 class as well as a PRS operator. In addition, the regeneration strategy for the community/commercial centre of the Wheat Quarter site – where the listed former factory buildings sit – has been further developed and refined.

The proposed scheme builds on the knowledge accumulated for previous proposals, yet its design brief is inspired by new ideas and objectives as well as by previously established principles:

- To transform the Shredded Wheat Factory site into a vibrant new destination with a distinctive character: The Wheat Quarter.
- To restore and re-purpose the original 1920s factory buildings.
- To create beautiful, durable, and useful buildings within an enjoyable setting.
- To provide a substantial amount of new homes in a range of sizes and increased diversification of the end user demographic
- To create public spaces that are well-connected, safe, inclusive, and enjoyable.
- To create places with quality and character, drawing primarily on the prominent location and industrial heritage of the Wheat Quarter site (hardscapes, mixed use, civic space) and on the potential for amenity provision (landscapes, biodiversity, environmental improvements).
- To create beautiful and enjoyable new landscapes, including the new sensory gardens designed especially for disabled people.
- To improve the quality, connectivity and accessibility of existing routes and infrastructure, with a focus on east-west routes and connections to the town centre and surrounding areas – including the pedestrian bridge over the railway.
- To benefit the local community by accommodating activities and services such as employment space, health and fitness and community services.
- To set out a vision for an arts centre and museum, investing in public art to be exhibited – indoors and outdoors – for the enjoyment of all.

1.10. DESIGN TEAM

COLLADO COLLINS ARCHITECTS

Since its formation in 2004, ColladoCollins has embraced the challenges of complex mixed-use design to create successful buildings and urban environments. ColladoCollins secures planning permissions through openness, sensitivity and tenacity to produce commercial schemes that are deliverable.

The practice operates across several sectors, combining a strong design ethos with sound technical strength through every stage of the design process. Unlike many firms who specialise in one particular area, ColladoCollins' unique approach ensures that its designs are informed by experience across a wide range of schemes.

ColladoCollins brings a unique approach to the process of designing and developing buildings, combining a full understanding of the commercial challenges faced by clients with a sensitive, sustainable work method. ColladoCollins works to ensure every building it designs perfectly fits its surrounding area in an environmentally conscious and visually arresting way.



SELECTED WORK



Bath Press

Location	Bath
Client	Spenhill
Contract Sum	£50m
Status	Planning Consented
Composition	244 Residential Units 15,000 sq.ft. Commercial

The development site is located in the City of Bath. It is currently occupied by the former premises of the Bath Press, built in various phases from 1889 to 1970, now vacant since 2007. It is proposed to retain some of the most iconic elements of the building such as the main historic façade and the chimney.

The proposals comprise 244 dwellings, 15000 sq.ft of commercial space and a total of 207 parking spaces.



Hillingdon Gateway

Location	LB Hillingdon
Client	Meyer Homes
Contract Sum	£73.4m
Status	Planning Consent
Composition	515 Residential Units 4,600 sq.ft. Retail 3,600 sq.ft. Offices

Despite a prominent location in Hillingdon and excellent transport links – next to Hillingdon station and the M40 – the Wheat Quarter site has been derelict after the Master Brewer hotel was demolished in the early 2000s. In 2016, Meyer Homes took over the Wheat Quarter site from Tesco and set a project brief calling for an intelligent design that responded to the sensitivities of the Wheat Quarter site, the surrounding area, and the local community, where there is a lack of affordable housing. ColladoCollins' design complements the existing interbellum and post-war housing stock and balances the needs of the local population with current London standards for building density, housing design, accessibility, and car parking.

Whilst refused locally this project has recently been called in and granted consent by the Mayor of London.



Brook Green

Location	LB Hammersmith
Client	Westerland Real
Estate	
Contract Sum	£30m
Status	Completed
Composition	83,000 sq.ft. refurbishment 56,000 sq.ft. new offices
Listing	Grade II

The former Ford Motor Company garage along Shepherd's Bush Road will be adapted to a new use, adding three new floors to the Grade II listed building housed in a striking glass dome. The design comprises over 115,000 sqft of office space across six floors and proposes the creation of a unique, high quality office space that incorporates contemporary design elements.

The design adds 50,000 sq.ft of new office accommodation above the existing building, and is a contemporary take on an historical precedent using modern diagrid technology.

BRADLEY MURPHY DESIGN

BMD is a landscape architectural practice founded and led by Laura Bradley and Ryan Murphy. We have a proven track record in designing and delivering high quality landscape and public realm for large-scale residential developments from inception through to completion.

Landscape is an evolutionary process shaped by the relationship between people and place. We believe that our role as landscape architects is as guardians of existing landscapes and as champions for the creation of new, high quality landscapes and public realm. Our placemaking approach is informed by an iterative, contextual design process that focuses on realising design vision and delivering places of enduring quality.

Our landscape-led approach to masterplanning creates added value by finding the right balance between spaces and built form, to create places where people love to live. Green Infrastructure is integrated at the heart of the masterplanning process and forms an essential part of our placemaking design approach. We are passionate about facilitating the sharing of new ideas and developing new methods of collaborative working to explore what landscape can do for people and place.

BMD was commissioned in 2013 to lead the public realm and landscape design working with the wider design team to develop the detail of the masterplan, ensuring a landscape-led approach to the shaping of the development. BMD are delighted to have been retained in the continued evolution of this unique development on the former Shredded Wheat site.



SELECTED WORK



Peruvian Wharf

Location: London
Client: Galliard Homes
Status: Planning
Composition: 950 Residential Units

BMD was commissioned in 2015 working alongside EPR architects to produce the landscape design proposals for a primarily residential development located in the Thameside West area, delivering c.1000 homes.

The proposal consists of a high quality residential-led, mixed use development with a well-connected public realm integrated into a unique townscape. The landscape proposals help to define a stronger site identity and landscape character that responds to the context of the local area and contributes to the development 's sense of place. Despite the challenges presented by the current industrial context, the Peruvian Wharf masterplan proposal maximising the opportunities that the vacant brownfield site offers, becoming an integral part of the Thameside West vision.



Tolworth

Location: London
Client: Meyer Homes
Status: Planning
Composition: 950 Residential Units

BMD was commissioned in 2016 to produce site wide proposals for the former industrial, brownfield Toby Jug and former Ministry of Agriculture site adjacent to Tolworth train station. The proposals comprise c.950 units mixed use development structured in a sequence of streets and spaces with varying and contrasting characters.

The concept for the appearance of the scheme draws from a number of influences including the defensive characteristics of medieval hill towns in response to the environment conditions of the A3 corridor. The landscape vision for Tolworth focuses on the creation of a place that fosters sustainable and lasting social, physical and environmental connections with neighbouring communities and the wider townscape and landscape context.



London Road

Location: Hounslow
Client: Meyer Homes
Status: Starting construction
Composition: 300 Residential Units

BMD was commissioned in 2015 to prepare landscape and public realm design proposals for a residential development of c.350 contemporary duplexes, apartments and commercial retail on London Road in Hounslow.

This exciting scheme drew on Woonerf principles, encouraging the use of mews streets as social spaces through the introduction of shared surfacing, integrated play, trees and feature planting.

Mews streets were combined with a sequence of publicly accessible spaces and communal gardens at ground & podium level to deliver interconnected green space across the development.



ICENI PROJECTS: TOWN PLANNING

Iceni is a town-planning led, multi-disciplinary consultancy. Currently in its 15th year, Iceni has expanded its regional presence beyond its Central London base through offices in Glasgow, Edinburgh and Manchester. Iceni is a modern, forward-thinking consultancy, partly reflected by the age of its team, but more relevantly, by its mind-set. Iceni is naturally attracted to complex and strategic scale developments that have the potential to achieve positive transformational change to communities, with homes, jobs and infrastructure delivered in tandem to create new sustainable development. The company has been tailored to formulate these projects and secure successful planning outcomes.

Iceni has extensive casework experience in Welwyn Hatfield and Hertfordshire. It has secured a major employment land allocation on the eastern edge of Welwyn Garden City through the draft Local Plan and is promoting numerous schemes for residential and mixed-use development across the County. Iceni's team is fully conversant with parking requirements, design policies and the approach of both Welwyn Hatfield District and Herts County to Planning Obligations from successfully promoting these projects.

Iceni's project of greatest relevance to the subject proposal is the redevelopment of the former GlaxoSmithKline and Lyons Bakery site at Greenford, West London. Iceni initially secured planning permission in 2014 for 593 new homes and a canal-side leisure hub including a cinema, food store, restaurants and cafes. The scheme preserved and restored the Grade II listed Glaxo House and included new public space, opening up the site for the first time in decades. The Greenford site was subsequently sold to the PRS operator Greystar Europe, who instructed Iceni to revise the scheme in order to achieve a significant intensification of the amount of floorspace and transform the regeneration opportunity. In March 2017, Iceni secured planning permission for 1,965 new homes to create the UK's largest purpose-built Build-to-Rent scheme. The similarities between the two projects site are striking, with the shared objective of reimagining and speedily delivering a stalled regeneration project through a comprehensive development.



ENTRAN: TRANSPORT, EIA

Entran is an independent environmental and transportation consultancy with specific abilities and expertise in traffic engineering, transportation planning, environmental impact assessment, air quality and noise assessments and monitoring. Entran was formed in 2005 on the premise of providing the highest quality advice, solutions and services to suit individual clients' needs and to provide high quality advice throughout the development lifecycle.

Entran has offices in Bristol and London and is the retained transport and environmental consultancy to numerous national firms and institutions operating across all sectors including, retail, residential, education, leisure, health, and energy.

Entran has specific experience in developing multi-modal access strategies and assessing the transport effects and traffic impacts, as well as the environmental effects of major new developments and regeneration projects.

Relevant recent projects include:
Bracklesham Bay (West Sussex) – 1,000 houses, new school, Park and Ride – Means of access/movement strategy to support LP allocation.
Keybridge House (LB Lambeth) – 590 dwellings, retail uses and new school – Transport Assessment and Travel Plans.
Medina Yard, Cowes (Isle of Wight) – Town centre regeneration comprising 535 dwellings plus 18,600m² non-residential uses (retail, food, museum, boat yard, employment) – Transport Assessment, Traffic Impact Assessment, Travel Plan.



EAME: DECONTAMINATION

EAME is a multi-disciplinary environmental consultancy practice that specialises in providing technical expertise and innovative solutions to complex and challenging environmental problems. The firm is based in the UK, Iraq, Kurdistan, Romania and Guyana which allows it to operate globally across a wide range of technical disciplines and industrial sectors. EAME's Environmental Scientists are a well-established team of environmental consulting professionals whose core members began working together over 15 years ago and have remained together as an integrated team ever since. Mr Steven Rowan (EAME MD) has over twenty-six year's environmental experience initially as a regulatory officer and technical specialist for UK Waste Regulation Authorities but for most of his career, in the private sector as a consultant to many multinational corporations from all industry sectors. Recent relevant project experience includes the redevelopment of a former coal tar creosote works (UK), regulatory negotiation associated with a residential development of a former petrol filling station (UK), and an environmental assessment of a former illegal waste depository (UK).



SOL ENVIRONMENT: ENERGY &

Sol Environment Ltd is an independent environmental, low carbon and sustainability consultancy that provides comprehensive and pragmatic advice in relation to commercial, residential, and industrial developments. The company's work focuses on Renewable Energy, Built Environment, Low Carbon Technologies and Environmental Permitting & Planning. Sol also offers a wide range of Corporate Compliance and Environmental Management services for the industrial, commercial and development sector.

Sol Environment specialises in the following services:
Planning Sustainability
Renewable Energy and Low Carbon Technology
Environmental Permitting and Regulatory Assistance
Verification and Operation Compliance
Business Sustainability and Environmental Management



WSP STRUCTURE & FIRE ENGINEERING

WSP was established in 1973 and, through strategic development, the company has grown into one of the largest international consultancy groups in the world. In 2012 we merged with Genivar, a major Canadian consultant.

In brief the combined WSP and Genivar company:

- employs around 14,500 staff in over 35 countries;
- generated a turnover in excess of £1.1Bn;
- operates in the property, environmental and energy, transport infrastructure sectors; and
- provides a full range of services.

Our regional offices offer an integrated engineering design and consultancy capability and we have a strong commitment to local communities and sustainability.

We offer an integrated team with a reputation for delivering award winning projects and facilities that are well designed, built on time, affordable, and properly maintained over their lives. We believe in designing once, and are committed to the timely production of correct information within the boundaries of a strong focus on value management. Quality of service is improved through knowledge sharing through engagement with experts across our business to validate and verify our designs. This process allows us to transfer efficient and innovative designs being used across business to the benefit of all of our clients.



J.S.H. IPSWICH MEP

At Johns Slater and Haward our people are our greatest asset. We say it often and with good reason - it is only with their hard work and effort that we are able to meet the needs of our Clients.

We have a strong commitment to staff development both through Vocational Training and wider Continuing Professional Development (CPD).

Chartered Engineers for the design of mechanical and electrical systems in buildings;
Registered with the Engineering Council;
Trained by the Building Research Establishment (BRE) for Building Regulations Part L (SBEM) Compliance;
Trained by TAS in the use of Daylighting and Thermal Modelling Software;
CIBSE Registered Low Carbon Consultants and EPC Assessors;
Trained in AutoDesk Revit for 3D drawing and Building Information Modelling (BIM)
A history of over 120 years operating as building consultancy practice;
Large enough to deliver on multi-million pound schemes, small enough to be flexible and personal in our approach;
Owned and operated by Director/Shareholders with a vested interest in Client satisfaction and repeat business;
Established relationships with Clients and contractors across all sectors of use and ownership.



MARENGO COMMUNICATIONS: PR

Marengo Communications is an independent communications and consultation consultancy working for sixteen years in the property sector, engaging with stakeholders and local residents on behalf of developers and their agents. Marengo is a member of the Consultation Institute and has strict ethical rules of engagement and conduct. The company's role is to coordinate and manage consultation and engagement with local residents, stakeholder groups, interested parties and the media. Marengo produces websites, exhibition boards, and the majority of correspondence on behalf of its clients, and often liaises with elected members from local authorities, other tiers of local Government, and Parliament.



ORSA: HEALTH & SAFETY

Orsa is an independent boutique compliance consultancy, specialising in all matters related to health, safety and environment. Orsa offers a broad range of compliance consultancy services in support of construction and development projects, film production, and day-to-day business delivery.

Orsa's roots are in construction and the built environment, with experience of small and major projects all over the world.

The construction industry is aware of the sensitivity surrounding health and safety issues on-site and its appalling safety record. It is against this backdrop that Orsa offers a wide range of tailored specialist safety services that can be commissioned individually, or as part of a fully integrated package, each designed to add value by optimising safety.



KMHERITAGE: HERITAGE ADVISOR

KMHeritage provides support on all aspects of the historic built environment, advising clients as they develop projects, negotiate with local planning authorities and English Heritage, and prepare material to explain and justify proposals in terms of national and local guidance on the historic built environment.

KMHeritage has worked on projects for major developments such as significant tall building proposals in London, Manchester, and Leeds. Examples include a successful planning application and listed building consent for the comprehensive conversion of the Royal Mint Court site into an employment-led mixed use development in the context of the conservation area, the setting of the World Heritage Site and the setting of nearby listed buildings; and support for the redevelopment of sites and conversion of various Grade II and Grade II* listed former mill and warehouse buildings within the Ancoats Conservation Area in Manchester.



MASONS: PLANNING SOLICITORS/RAILWAY

Masons Property Advisers is a multidisciplinary Chartered Surveying Practice established in 1990. Over that last 27 years it has built up a deep understanding of the property development process. Masons expertise extends to the feasibility assessment of complex schemes, sustainability, viability, planning and ultimately, delivery. Within this context, Masons provides specialist property-related advice on projects within the Railway environment and has enabled the delivery of over 4 million square feet of development within this challenging and unique sector of the market.

THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

2. ASSESSMENT

2.1. HISTORICAL BACKGROUND

GARDEN CITY PRINCIPLES

In 1898, the English reformer and urban planner Ebenezer Howard published *To-morrow: A Peaceful Path to Real Reform* (re-printed in 1902 as *Garden Cities of To-Morrow*). The book described Howard's vision for the 'Garden City': "to bring the best elements of both urban and rural life together into a new, revolutionary way of living". This concept is illustrated in Howard's famous 'Three Magnets' diagram, which lists the perceived advantages and disadvantages of 'Town' and 'Country' – the traditional living environments depicted by two magnets. The third magnet, 'Town-Country' has all the combined benefits of the two traditional, yet diverse models of living, but none of their disadvantages (such as the overcrowded living conditions of towns or the poor social facilities and lack of job prospects of the country).

The intention of blending the best of both city and country into a new way of living was the basis of the Garden City, whose key characteristics, as formulated by Howard, included:

- A fair distribution to the community of the profits that result from new development, founded on land value capture and the control of core utilities;
- Strong political support and leadership, with firm commitment to a clear vision and community participation;
- A suitable body to manage community assets over the long term;
- A full range of employment opportunities within easy commuting distance of homes;
- Beautifully and imaginatively designed, high-quality homes with gardens, combining the best of town and country living to create healthy living environments;
- Development that enhances the natural environment, providing net biodiversity gains and using zero-carbon and energy-positive technology to ensure climate resilience;
- Strong cultural, recreational and shopping facilities in walkable, vibrant sociable neighbourhoods; and
- Integrated and accessible transport systems, with walking, cycling and public transport designed to be the most attractive forms of local transport.

Howard's vision for this new way of living attracted enough financial support to found Letchworth, the first Garden City. Construction work began in 1903 under the stewardship of Raymond Unwin and Barry Parker, whom Howard trusted to implement his vision.

After construction started in 1920, Welwyn Garden City was the second and last true Garden City to be built in England to this date. As was the case with Letchworth, Howard's Garden City principles were implemented by another person – this time the architect Louis de Soissons.

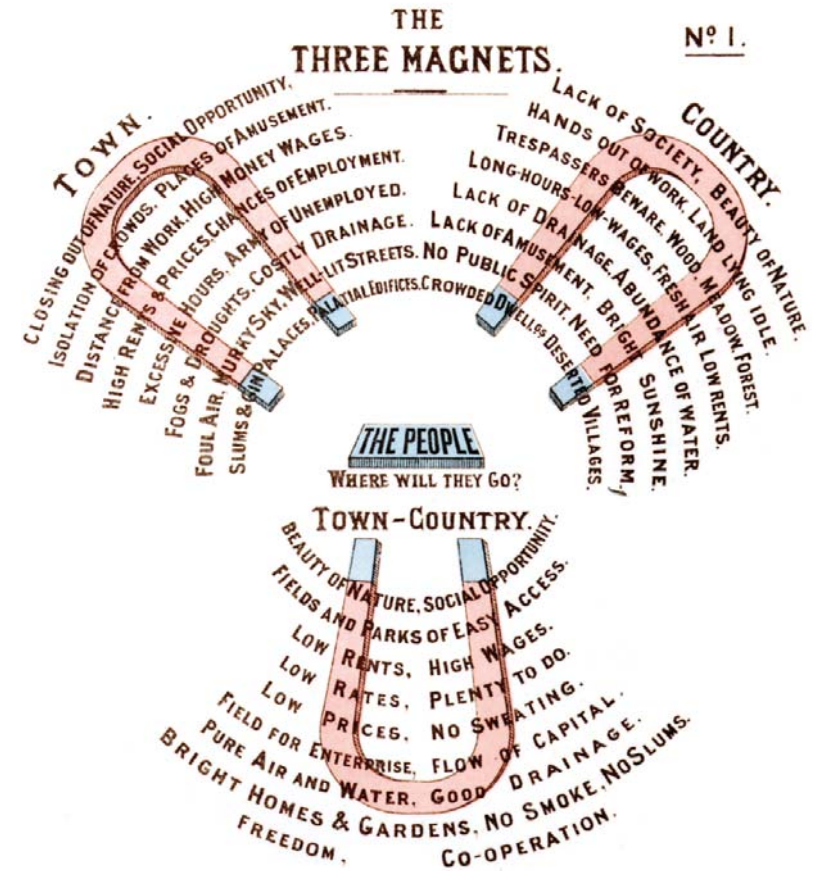


Diagram No.1: The Three Magnets (in Ebenezer Howard, *To-morrow: A Peaceful Path to Real Reform*. 1898, Swan Sonnenschein & Co.)

WELWYN GARDEN CITY PLANNING HISTORY

Louis de Soissons was born in French Canada in 1890. He trained at the Royal Academy and at the École des Beaux-Arts in Paris. In April 1920 he was appointed as the principal architect and planner for a new Garden City for 40-50,000 inhabitants.

His employer, Welwyn Garden City Limited, had been formally established as the owner and developer of an 2,378 acre estate in Hertfordshire.

Welwyn was de Soissons's first major commission. To deliver it, he set up a practice – the Louis de Soissons Partnership – with George Grey Wornum, with whom he presented a master plan for the new city in June 1920.

Louis de Soissons and the partnership were actively involved in Welwyn Garden City's planning, architectural design and growth for over 60 years. The development company – Welwyn Garden City Limited – allocated land for industrial and business purposes, as well as for privately owned houses, granting leases with restrictive covenants. De Soissons had total control of the plan of Welwyn Garden City – all plans of buildings had to be submitted to him under the provisions of the Building and Other Regulations in Force on the Welwyn Garden City Estate of 1923. Welwyn Garden City Limited successfully controlled the appearance of the town, and built about 40% of the buildings.

The new Garden City was to be self-sufficient – a carefully planned settlement combining industry and agriculture into a distinct whole; a self-contained community with an industrial area providing jobs in 'clean' manufacturing industries.

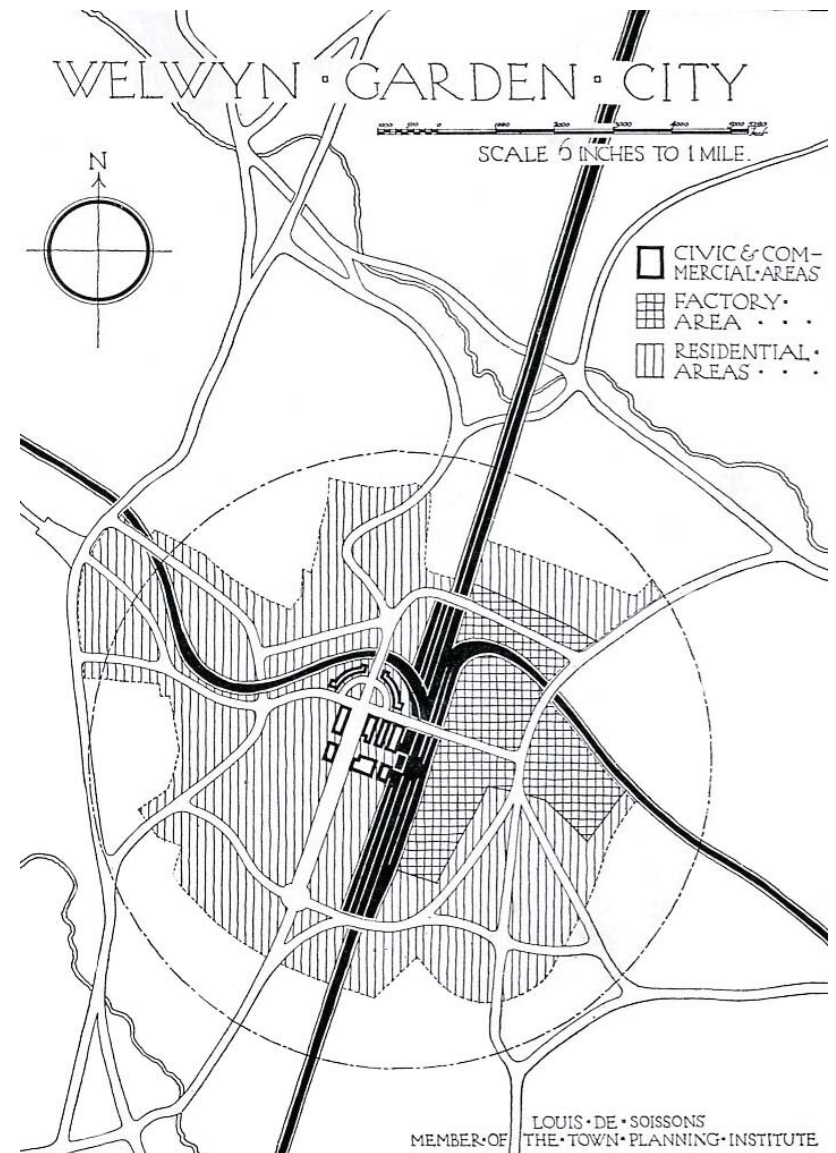
This self-containment was meant to prevent the town becoming merely a dormitory town for London. The limits on its growth were ultimately set by the surrounding green belt and any trees already in the landscape were retained in the initial plan. A follower of Garden City philosophy wrote in 1911:

'The object of a Garden City is to draw away from overcrowded localities or to intercept the ever increasing flow from the country by establishing new industrial towns in the country: towns which shall always stand in their belt of agricultural land' (Sir Ralph Neville, 'Garden City and Garden Suburb', in Garden Cities and Town Planning, n.s. Vol. 1, No. 1 (1911).

To the west and north of the town centre were residential districts, and to the east of the railway line were areas reserved for industry and business, beyond which were further areas of housing. The industrial area was beside the London and North Eastern Railway (LNER) and the branch line to Hertford which swept north-eastwards (now closed). The fairly level land close to the railway was recognised as the best site for industry. Most of the factories were eventually located along Broadwater Road, which stretched in the north-south direction.

Since the introduction of Permitted Development rights, Welwyn Garden City's industrial area is undergoing an important transformation – from a predominantly industrial, commercial zone to a mixed use, active part of the town.

At the heart of this transformation, the former Shredded Wheat Factory site provides an opportunity for a new vibrant place east of the town centre.



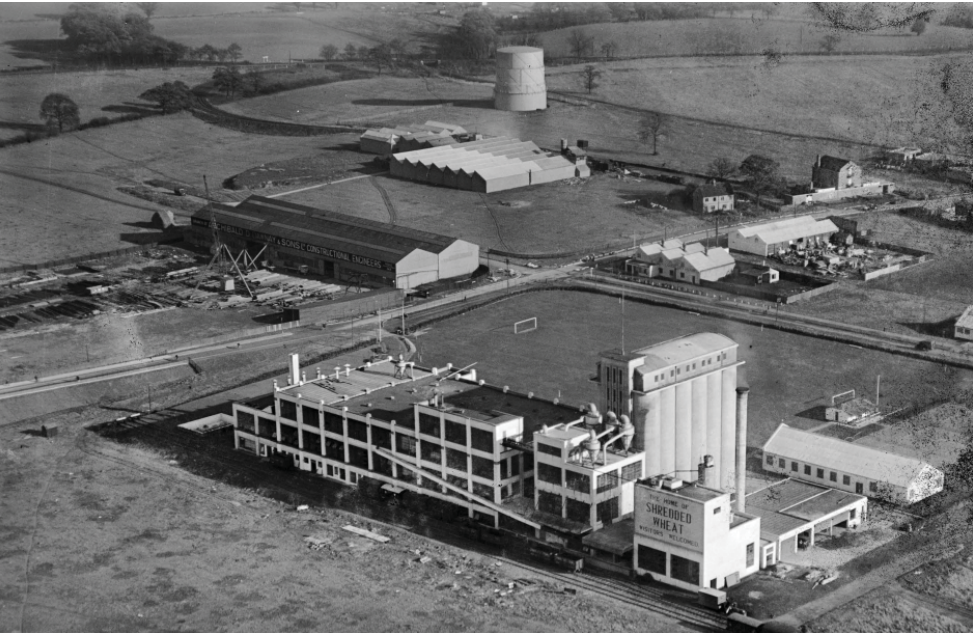
Welwyn Garden City master plan. Drawing by Louis de Soissons.

HISTORY OF THE APPLICATION SITE

The first company to occupy a site in Welwyn Garden City's new "Factory Area" was Archibald D. Dawnay & Sons Ltd, construction engineers and structural steel fabricators, who took a site near Hunter's Bridge (a bridge over the railway). They were soon followed by the American Shredded Wheat company, which leased seven acres just to the south in an excellent position, directly adjacent to the railway lines.

Shredded Wheat was attracted to the town for several reasons – the close proximity to London, without the 'smoke, grime and fog, that London is apt to indulge in'; and the convenience of getting 'suitable railway siding accommodation for receiving goods to other parts of the country' (Hertford Local Studies Library, Industry Moves Out (n/d), p.19).

Equally important to the location near the railway was a healthy environment where the factory workers could profit from all the social and economic benefits of 'the happy Shredded Wheat Family'. The company believed that 'all food for human consumption should be manufactured under the most rigid sanitary conditions and amidst surroundings that are cleanly, healthy and pleasant'. Welwyn Garden City was chosen as 'more nearly conforming to these ideals than any of the other sites offered'.



Aerial view of the original Shredded Wheat factory buildings circa 1928



Signage facing the railway



Production Hall – original east elevation



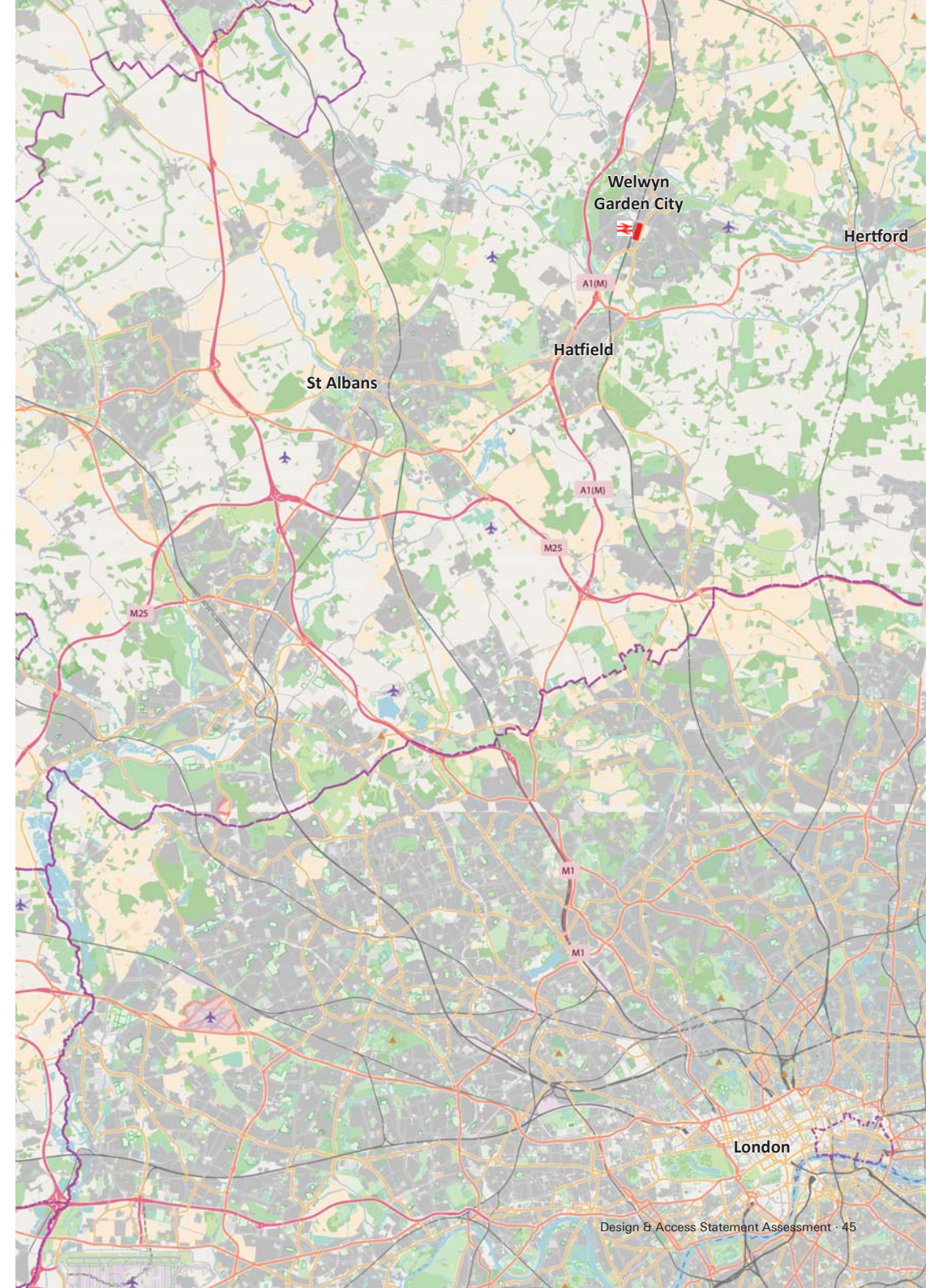
Production Hall interior

2.2. WIDER CONTEXT

Welwyn Garden City today is a town with a population of 116,000 (ONS estimate mid-year 2014) and a strong commercial base. Companies such as Ocado, Roche, Tesco, and Xerox are based there. HSBC has a high-security global data centre in Welwyn. The A1(M) motorway and the Great Northern railway provide fast connections to Central London (20-25 minutes to King's Cross, for a 40km distance), as well as to Cambridge and the North. Other trains connect the town with local stations through the north London suburbs. London Luton Airport is 13 miles away, London Stansted 30 miles, and London Heathrow 33 miles away – all are easily accessible from Welwyn. Other local destinations for employment and housing – besides London – are within short drives, particularly St Albans and Stevenage.



Above: View of Howardsgate, one of the primary boulevards in the centre of Welwyn Garden City.
Opposite page: Welwyn Garden City in the local Hertfordshire and Greater London context.



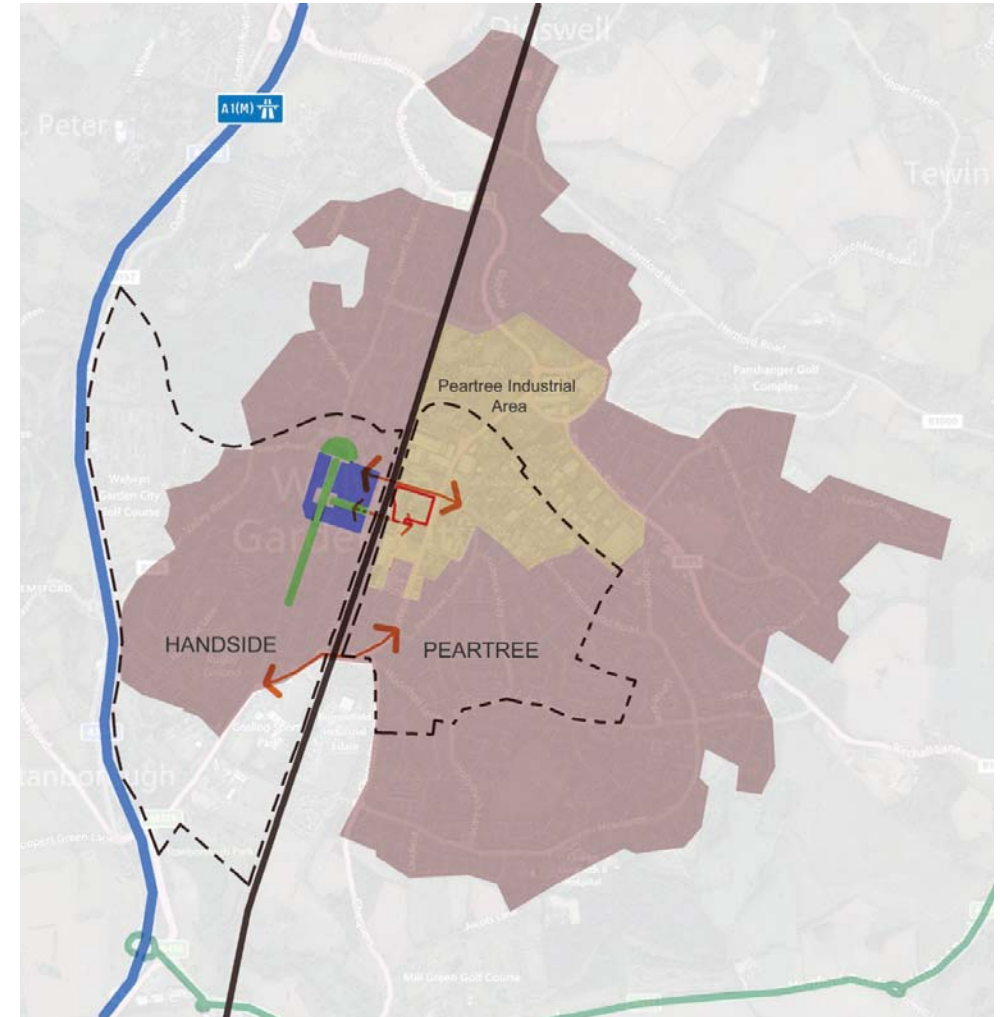
2.3. LOCAL CONTEXT

WELWYN GARDEN CITY AND THE APPLICATION SITE TODAY

Despite substantial growth around the industrial areas and the construction of the Howard Centre shopping mall in the 1980s, Louis de Soissons' master plan for Welwyn Garden City can easily be recognised, especially in the city's commercial centre.

The original zoning also remains, with the railway still separating the industrial areas from the residential areas around the Parkway. This means that the Peartree neighbourhood, east of the railway, remains noticeably separated from the city centre and from the residential areas west of the railway. The application site lies just east of the railway – along the separation line – and so presents an opportunity to improve the connectivity between two currently separate parts of Welwyn Garden City.

Opposite page: Welwyn Garden City local context.



THE APPLICATION SITE IN CONTEXT

The application site is bound by Bridge Road to the north, Broadwater Road to the east and Hydeway to the south and visible from the railway.

It is still well known as the factory site of the popular Shredded Wheat breakfast cereal.

The application boundary encompasses approximately 11.71 acres (47,410m²) of level ground. A short spur road links to a footbridge that connects the Wheat Quarter site to the Railway Station and to the Howard Centre shopping mall on Howardsgate, which serves as a gateway to the town centre.

The recent introduction of Permitted Development rules facilitated the conversion of non-residential buildings

(such as the former Xerox offices) to residential use, therefore transforming the character of certain sectors of Welwyn Garden City. A new mixed-use development (in accordance with local planning guidance) will continue a pattern of change of use in some formerly non-residential sites. This particular location – adjoining the railway station and the thriving town centre – is potentially very appealing to future residents and visitors.



Local context of the application site

CONSERVATION AREA AND LISTED BUILDINGS

Although the application site is not located within a conservation area, it is in close proximity to the Welwyn Garden City Conservation Area (see diagram on opposite page), which covers a large portion of the town centre (to the west of the Wheat Quarter site) and its residential hinterlands within the ward of Handside.

There is a wide separation between the development site and the conservation area, due to the railway lines with associated sidings, and the Howard Centre's large bulk, all of which sits between the Wheat Quarter site and the town centre. Nevertheless, the design team has considered the potential effects of the design proposal upon the conservation area – in particular through distant views. The potential effect of the proposed design on existing views from locations within the conservation area are assessed within BMD's Townscape and Visual Impact Assessment, under separate cover.

As illustrated by the plan on the opposite page, there are two groups of listed buildings located within the application boundary or in close proximity to the Wheat Quarter site. The listed buildings within the application boundary (near its northern edge) were commissioned by the American Shredded Wheat Company and designed by Louis de Soissons. They include the original Production Hall and the original cluster of wheat silos built in 1925, all of which have a Grade II Listing (as do the more recent

buildings added during the 1930s and 1950s to support the factory's growth). All listed buildings have been vacant since the relocation of Cereal Partners UK in 2008.

Historic England List Entry Summary:
Grade: II
Date first listed: 16-Jan-1981

1925. Architect Louis de Soissons. Two concrete ranges, at right angles with links. Southern range consists of giant range of cylindrical concrete drums 15 bays long with flat oversailing capping with railings right over the whole top. Behind this is a plain attic storey with 28 plain windows with plain capping over. On one end elevation is a 3 bay projecting tower rising just above the main roof level.

At the west end of the range is a 2 bay wing with large windows, the southern bay of 3 storeys and the northern of 4. Flat roofs. Adjacent is a 7 bay, 4 storey block, with large windows divided by narrow piers and small scale structural divisions between the storeys, making it almost wholly glass. Flat oversailing capping at roof level.

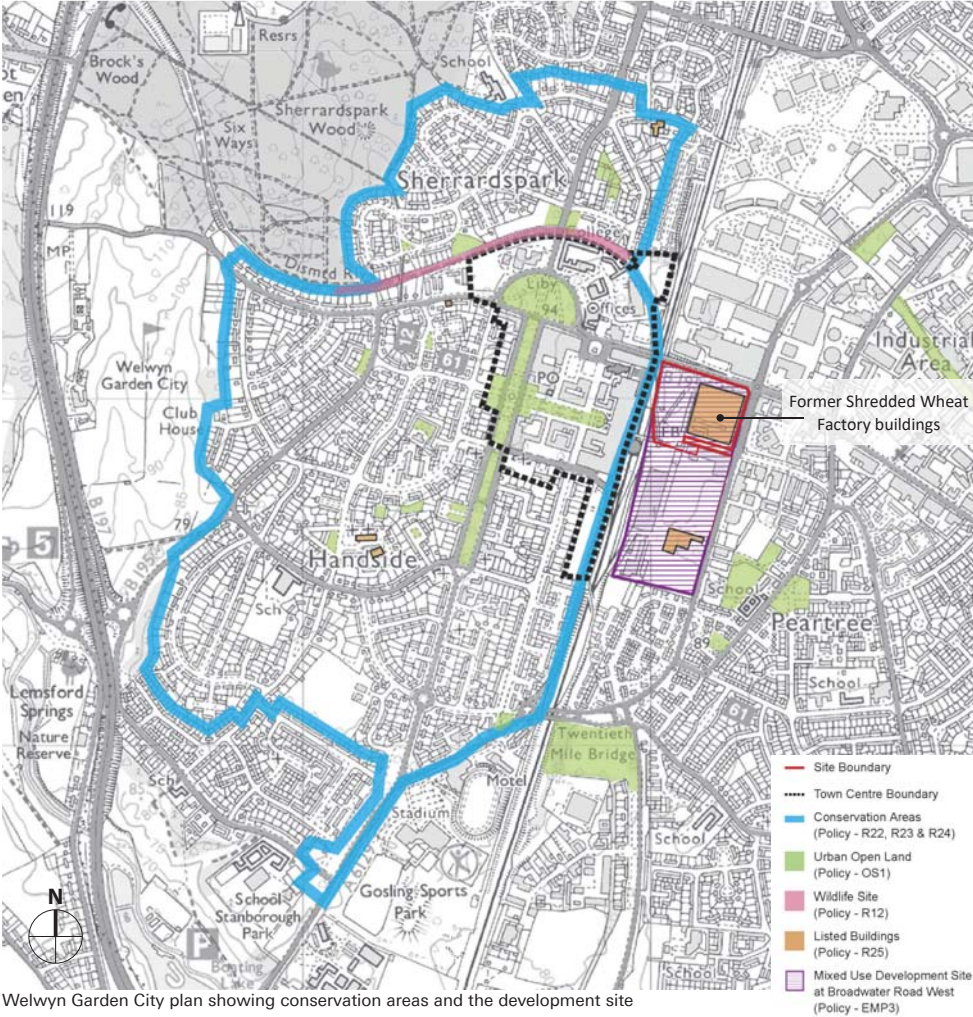
Listing NGR: TL2417112968



View from Parkway (within the conservation area) looking towards the Wheat Quarter site. The Howard Centre is visible in the middle.



View of the former Shredded Wheat Factory buildings



2.4. PLANNING CONTEXT

PLANNING HISTORY

The Wheat Quarter site, located to the east of Welwyn Garden City town centre, is bisected by Hydeway, which stretches east-west. North of Hydeway is the location of the former Shredded Wheat breakfast cereal factory; to the south of Hydeway the land was used for other factories and industrial facilities. The Wheat Quarter site is redundant and has been vacant since 2008. The Application Site is approximately 4.74ha. The Former Shredded Wheat factory buildings (including the silos) are Grade II listed.

The surrounding area comprises of a variety of uses and building types, including low level industrial and commercial/business uses to the north and east, the remainder of the Wheat Quarter site to the south with the first phase of affordable housing under construction, and Welwyn Garden City Train Station and Town Centre to the west. Further beyond the Site to the east lies the eastern half of Welwyn Garden City, largely comprising residential neighbourhoods and a large-scale employment area spanning over 100Ha. The site is strategically important, given it forms a gateway to the eastern part of the town for visitors from the railway and a key gateway to the town centre for residents on the eastern side of the town.

Since 2010, a number of planning applications have been submitted proposing to redevelop the site. An application (ref: N6/2015/0294/PP) for mixed-use redevelopment of the Site was approved in 2017, along with an accompanying Listed Buildings Consent (ref: N6/2015/0293/LB). The relevant pre-demolition conditions imposed on this Listed Buildings Consent were cleared to allow demolition to take place in accordance with the scheme approved in application ref: N6/2015/0294/PP. Demolition of the buildings detailed has taken place.

Application ref: 6/2018/0171/MAJ was approved on 15th February 2019 by Welwyn Hatfield Borough Council's Planning Committee, for the following:

'Creation of a mixed-use quarter comprising the erection of up to 1,340 residential dwellings including 414 (31%) affordable dwellings (Use Class C3); 114 extra care homes (Use Class C2); the erection of a civic building comprising 497m2 of health (Use Class D1), 497m2 of community use (Use Class D1), 883m2 of office (Use Class B1) and 590m2 of retail (Class A1/A2/A3/A4/A5); alterations, additions and change of use of Grade II Listed Building and retained Silos to provide 5,279m2 of flexible business floorspace (Use Class B1), 270m2 Combined Heat and Power (Sui Generis), 2,057m2 International Art Centre (Use Class D1), 1,235m2 Gymnasium (Use Class D2), 1,683m2 of Restaurant/Coffee Shop/Bar (Use Class A1/A3/A4/A5), Creche/Day Nursery (Use Class D1) of 671m2 as well as a Network Rail TOC Building (Use Class B1) of 360m2, plus associated car parking, access, landscaping, public art and other supporting infrastructure.'

The relevant clauses of the Section 106 agreement for application ref: 6/2018/0171/MAJ have been satisfied, and the relevant pre-commencement conditions have been discharged. Construction has begun at the southern end of the southern part of the site (beyond the current planning application site) for affordable housing blocks.

Subsequent planning permission has been granted in 2020 for extensions and improvements to the retained original 1920s silos, production hall, grain store and boiler house to enable the creation of an International Art Centre, including enhanced connectivity and access, and flexible uses within Block 4 and Block 5 (B1/D1/D2 Use Classes).

Further information on the planning history for the Site is included in the Planning Statement.

DESIGN GUIDANCE

Regarding planning policy, the application site (and other adjoining sites) are covered by the Broadwater Road West Supplementary Planning Document (BRW SPD), adopted by Welwyn and Hatfield Borough Council in December 2008. The BWR SPD sets out a vision for the Wheat Quarter site and key objectives as follows:

'To deliver an energetic and pioneering scheme of development which integrates the spirit of the garden city with the very best of high quality 21st century design, seizing the opportunity to enhance the local environment and create a sustainable, supported neighbourhood of an appropriate scale, which successfully integrates with the local community.'

This vision is based on the following objectives:

- To create a sustainable neighbourhood with an appropriate mix of uses for its central location;
- To establish strong connections between the east side of town, the Wheat Quarter site, and through to the town centre;
- To use the industrial heritage as a cue for form, character and identity - re-use listed structures and ensure sympathetic development;
- To explore innovative and sustainable uses drawing on the natural, infrastructure and spatial assets of the Wheat Quarter site;
- To create urban grain capable of supporting appropriate uses while reflecting the town centre morphology;
- To consider accommodating large scale uses which would best be located near the centre as the opportunity to do this is unlikely to arise again;
- To support live-work proximity;
- To define the amount of employment land/floor space that should be provided for;

- To enhance biodiversity.

This Design and Access Statement aims to articulate a feasible vision and design for the application site, reflecting and building upon the vision and objectives set out in the BRW SPD masterplan.

The following documents have been taken into consideration for the design of the master plan and general layout of the residential and commercial elements: Welwyn Hatfield District Plan. Supplementary Design Guidance, February 2005.

Secured by Design. Homes, February 2016
Welwyn Hatfield District Local Plan adopted 2005 and emerging Welwyn Hatfield District Local Plan 2017

2.5. CONSENTED SCHEME VISUALS

A number of rendered CGI's were produced to accompany the last submission for the Wheat Quarter. We have sought to replicate view locations and CGI's for comparison as part of the Design section of this document.



2.6. ADDITIONAL DEVELOPMENT

SUMMARY COMPARISON TABLE

Component Part	2019 Consented	2020 Proposed	Difference	Notes
Class C3 Residential	697	560	-137	Reduction due to change in tenure (increase in residential care and introduction of PRS component)
Class C3 PRS	0	399	+399	Significant PRS component incorporated for tenure diversity and immediate delivery
Class C2 Residential Care Community	114	250	+136	Increase required to secure leading market operator and diversity of tenure to drive delivery
Non-Residential	12,700*	15,000	+2,300	Additional 1,131sq.m floorspace proposed through Community Bridge Building to incorporate Cycle Hub and Leisure/Business Uses Additional floorspace proposed in the form of small non-residential pods within Residential Blocks 1 and *1,200sq.m consented through 2019/1347/FULL for additions and improvements to Listed Building and 11,503 through 2018/0171/FUL
Parking	695	742	+47	600 residential and 142 non-residential spaces.

THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

3. INVOLVEMENT

3.1. SUMMARY OF CONSULTATION PROCESS

Following the extensive public consultation and stakeholder engagement programme undertaken as part of the earlier proposals for The Wheat Quarter, we have remained committed to ensuring the public and key stakeholders are aware of our plans. Since the approval of the initial scheme in 2018, we have kept in touch with the community, via regular meetings, e-bulletins and updating the website for the project. During the process of designing our revised proposals, we have had a series of pre-application meetings with officers at Welwyn Hatfield Borough Council, as well as meeting with the ward members for Peartree Ward. In addition to this, our project team attended an all member meeting to present their plans for the north of the Wheat Quarter site.

Upon submission of the application we will be updating our website with the updated plans, issuing an e-bulletin to our mailing list and holding two online public webinars where we will be presenting our plans and taking questions.

3.2. PRE APPLICATION

PRE-APPLICATION CONSULTATION, MAY 2020

The proposed scheme has evolved considerably since our Pre-Application meetings.

Whilst the scheme is based on a new development brief, it builds on important principles that underlie the Masterplan Framework and the consented scheme – particularly east-west connectivity, the grid form, preservation strategy, land use, and the palette of architectural materials.

The main elements of the landscape design concept – character zones, courtyard spaces – have also been carried through. Nevertheless, the new development brief introduces new ideas and strategies which improve on the consented scheme, for example: the introduction of two new axial east-west streets that facilitate new views

of the Listed Buildings and further increase permeability across the Wheat Quarter site.

Following the proposed amendments, the Pre-App scheme proposed the following accommodation.

	Proposed	Consented
Blocks 1: C2 Units	80	-
Blocks 2: C3 Units	225	149
Block 3 C3 Units:	225	114
Block 6E: C3 Units	68	28
C3 Units	320	239
Blocks 7: C3 Units	400	281
TOTAL: Units	720	520



SUMMARY OF PRE-APPLICATION CONSULTATION



3.3. PRE APP COMMENTS & RESPONSE

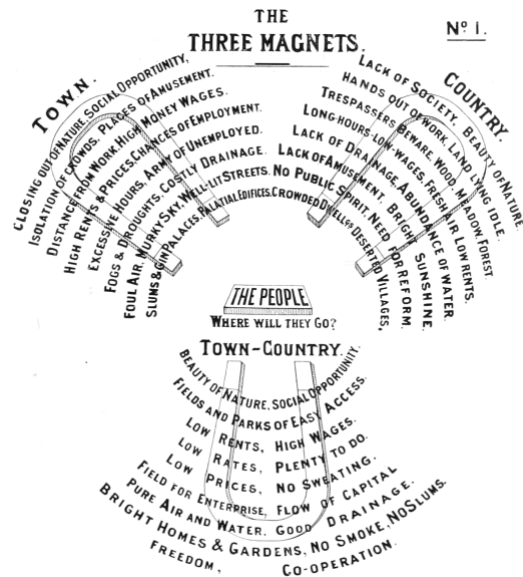
Issue	Key comments	Response
Heritage Assets	<i>The proposed raising of the heights of the blocks will undermine these aspects of the listed building's setting. I have concerns that the proposed 13 storey blocks will be particularly detrimental in introducing competing tall buildings of greater massing than the silos. There are also concerns that the increase in building heights immediately surrounding the listed building will have a crowding effect and undermine the opportunity to create a new, enhanced public space with the listed building as the focal point. For this reason, the increase in height, particularly those of 13 storeys, are objected to.</i>	<p>The Silos are approximately 11 residential storeys in height. We have reduced the proposed massing to in order to minimise any impact on the heritage assets and sit below the height of the Silos.</p> <p>Block 6 has been reduced to be a maximum of 10 storeys on the junction of Broadwater and Bridge Road and minimum of 5 storeys opposite the Production hall.</p> <p>Block 7 has been reduced to a maximum of 10 storeys on the gateway into Hydeway and a minimum of 5 storeys opposite the Production hall and Silos.</p> <p>West of the heritage assets Reiss walk has been increased in width in order to accommodate proposed buildings ranging from a maximum of 8 storeys against the railtracks to a minimum of 5 storeys for Blocks 2&3.</p> <p>Block 1, which serves as the termination point of Hydeway and therefore has a prominent place on Goodman Square, is proposed as a storey taller at a maximum of 9 storeys.</p>
Urban Design Block 6	<i>There is the opportunity to make Block 6 a strong perimeter block by infilling above the access route into the centre, provided that the internal amenity is not adversely affected by shadowing.</i>	<p>In order to further enhance the important urban edge this element of the scheme holds we have widened the courtyard to this building, by pushing the northern core further north to create a frontage on Bridge road, whilst retaining the extant symmetry and footprint with the reinstated Production hall entrance and approach road.</p> <p>We have not infilled the southern break in the massing as this would have a detrimental effect on daylight into the courtyard.</p>

Issue	Key comments	Response
Urban Design Blocks 1,2&3	<i>The proposal includes gable fronted 1-3 storey elements on the upper levels of some blocks which have been designed to allow for views towards the listed assets. These are considered inappropriate for this development. These features should be removed as they are at odds with the overall design.</i>	<p>The architectural design of Block 1 has been developed in light of the Pre-App comments received and forms part of the Detailed Planning Submission.</p> <p>Whilst Blocks 2&3 are being submitted in Outline we have sought to rationalise the massing and design intent in response. The Outline submission elements and their design intent is further explored within the submitted Design Code.</p>
Urban Design	<i>There does appear to be good consideration of design detail in the scheme, however the primary concern at this stage is that the architecture could appear as being 'anywhere'. The Garden City has very distinctive architecture and details/features which should be used as a contextual reference for this scheme.</i>	As part of the design development we have made reference to our analysis of the 'industrial' past of the Wheat Quarter site in contrast to that of the South Side.
Residential Amenity	<i>In relation to private and communal amenity, apartments are all shown with a minimum of 5sqm private amenity space, which is supported. Clarification is however required that these apply to all apartments within the north site without exception.</i>	All apartments will benefit from private amenity spaces in excess of 5m2 in the form of gardens, balconies and terraces.
Overlooking & Privacy	<i>There is concern regarding the proximity of development particularly the elevations which are separated by 16m in distance.</i>	<p>Where residential single aspect habitable rooms are face to face facades are a minimum of 20m face to face.</p> <p>In the few cases where these dimensions are compromised the residential units facing each other are dual aspect and as such we have ensured that the primary aspect has in excess of 20m with only the secondary aspect being reduced. Careful consideration has been given to the design of facades and in particular placement of windows in order to avoid overlooking or even the loss of privacy.</p>

THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

4. EVALUATION

4.1. GARDEN CITY DESIGN PRINCIPLES



The Town and Country Planning Association (TCPA), founded by Sir Ebenezer Howard in 1899, launched the Garden Cities movement. It originated Letchworth in 1903 and Welwyn in 1920, and carries Howard's legacy of progressive ideas to this day. The TCPA defines a Garden City as "a holistically planned new settlement which enhances the natural environment and offers high-quality affordable housing and locally accessible work in beautiful, healthy and sociable communities".

The Garden City principles have been interpreted for a 21st century context, to include:

- Land value capture for the benefit of the community.
- Strong vision, leadership and community engagement.
- Community ownership of land and long-term stewardship of assets.
- Mixed-tenure homes and housing types that are genuinely affordable.
- A wide range of local jobs in the Garden City within easy commuting distance of homes.
- Beautifully and imaginatively designed homes with gardens, combining the best of town and country to create healthy communities, and

including opportunities to grow food.

- Development that enhances the natural environment, providing a comprehensive green infrastructure network and net biodiversity gains, and that uses zero-carbon and energy-positive technology to ensure climate resilience.
- Strong cultural, recreational and shopping facilities in walkable, vibrant, sociable neighbourhoods.
- Integrated and accessible transport systems, with walking, cycling and public transport designed to be the most attractive forms of local transport.



The emerging proposal addresses the Garden City Design Principles within the context of a 21st century development with increased density.

4.2. LISTED BUILDINGS

SHREDDED WHEAT FACTORY HISTORY

The Grade II listed former Shredded Wheat Factory and original cluster of 18 silos have been a distinctive symbol for Welwyn Garden City for over ninety years. The factory site, rectangular in plan, is situated at the northern end of the masterplan site and is bordered by Hunters Bridge, Hydeway and the main railway line that runs through the heart of Welwyn Garden City. The factory has remained vacant since 2008 when Shredded Wheat moved their production to Staverton, Wiltshire.

The 1920's factory was designed by the principal architect for Welwyn Garden City, Louis de Soissons. The factory development has evolved and expanded over its lifetime, reflecting the corporate expansion of the former occupiers, Shredded Wheat. The nine buildings within the complex have been constructed over three distinct periods. This allowed Shredded Wheat to supply the increasing demand for breakfast foods during the early 20th century.

The Shredded Wheat Factory was a symbol for Welwyn Garden City and for de Soissons due to its revolutionary modern style; a dramatic contrast to the Georgian Arts & Crafts architecture found elsewhere in the town. The Factory was Shredded Wheat's first European manufacturing premises and the design was based around their flagship factory in Niagara Falls. The 3 storey complex, excluding the silos, was built in a predominantly functional architecture of painted concrete and rendered walls with Crittall metal windows. The factory was also one of the first buildings in Britain to be built utilising the flatslab construction method, a technique that had been developed in Europe and America.

The development of the former Shredded Wheat factory can be summarised in three main stages.

1926 - 1928

The completion of the original factory complex for Shredded Wheat which included the production hall, boiler house, eighteen silos, wheat elevator and garage.



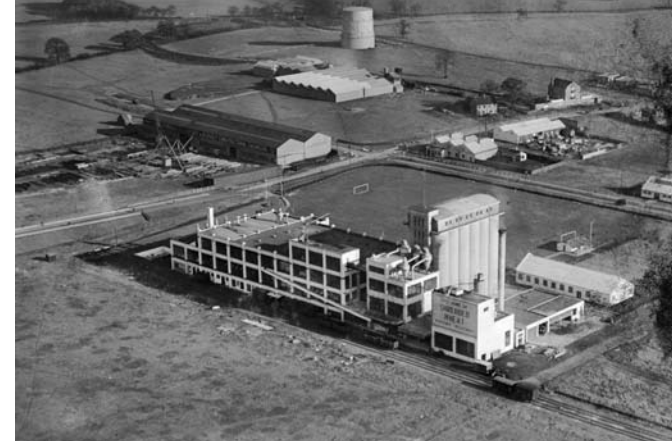
1937 - 1939

A new single storey was fixed to the east of the factory and also on the roof of the multistorey original production hall. The number of silos also increased from 18 to 45. The additional 27 storage bins were designed by Louis Wirsching Jnr (the Nabisco company architect) which increased capacity to 8,500 tons. The additional silos were a separate, independent structure to the original 18 silos, but appeared visually to be fully integrated.



1950s

From 1941 to 1947 the Government introduced a zoning scheme which excluded Shredded Wheat from selling in certain areas; limiting production. Once this zoning came to an end, the company steadily increased its market share. A large extension on Broadwater Road increased production facilities, but reduced the recreation grounds. In 1957 & 1959 two further extensions were added, improving production and providing increased administrative facilities.



Aerial view of the original Production Hall, Boiler House, 18 silos, grain elevator, and garages



Aerial view of Welwyn Garden City circa 1941. The Shredded Wheat factory is on the lower left.

4.3. SITE-WIDE ACCESS AND CIRCULATION

Existing Connections

The Wheat Quarter site is currently accessible from the west through a footbridge that connects the Howard Centre to the train station and extends over the railway into the Wheat Quarter site ①, with steps down to ground level at Hydeway, which crosses the Wheat Quarter site and intersects Broadwater Road on the east boundary of the Wheat Quarter site ②. Hydeway is accessible to vehicles as well as to pedestrians, but is currently a dead-end road (with site access only). The footbridge-Hydeway east-west link, which splits the Wheat Quarter site (into North Site and MTVH Site) is the main existing line of access and circulation.

On the north, an existing slip road facilitates vehicular access to the PDW Gates Distribution warehouse. A new pedestrian route is to be created linking the front of the Production Hall with Bridge Road and beyond ③.

The MTVH Site is currently fenced-off. There are no remaining roads or pathways.

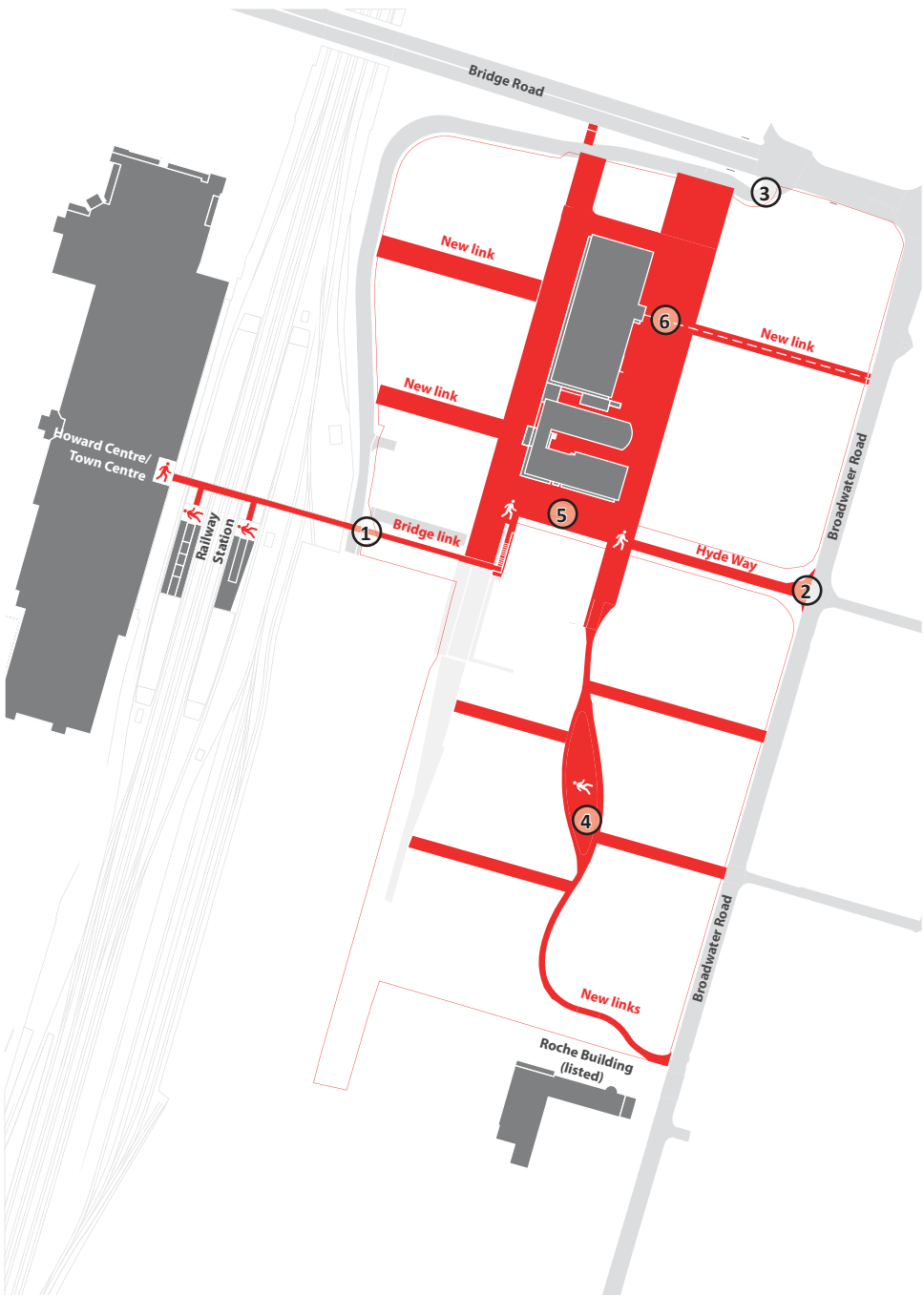
New & Enhanced Connections

Because only one east-west connection currently exists along a distance of circa 430 metres, it is essential to create new east-west links through the Wheat Quarter site – and to reinforce the existing one, which is a primary link between the town centre and the Peartree area.

New east-west connections will divide the Wheat Quarter site into smaller areas and establish the approximate outlines of buildings/blocks. In addition, new points of access and desire lines will be created. With an average width of 150 metres, the MTVH Site can accommodate two rows of building blocks, with a pathway in between. A new pathway/desire line ④, running north-south, serves as a central spine, as an extension of new public spaces around the factory buildings, and as a link between listed buildings – the former Shredded Wheat factory and the Roche building south of the application site.

On the north, improved connections to Bridge Road extend along the factory buildings and provide new public spaces appropriate to the buildings’ monumental scale, and designed to expose the Production Hall’s main elevations (east and west), as well as opening a central plaza ⑤ flanked by the grain silos. Also on the North Site, a new access road will be aligned with the main east entrance ⑥ of the restored Production Hall, making the main entrance visible and accessible from Broadwater Road.

On the north, improved connections to Bridge Road extend along the factory buildings and provide new public spaces appropriate to the buildings’ monumental scale, and designed to expose the Production Hall’s main elevations (east and west), as well as opening a central plaza ⑤ flanked by the grain silos. Also on the North Site, a new access road will be aligned with the main east entrance ⑥ of the restored Production Hall, making the main entrance visible and accessible from Broadwater Road.



4.4. SITE LAYOUT

PERMEABLE LAYOUT

The Masterplan Framework in the Broadwater Road West SPD is based on the principles of grid form and permeable layout. It consists mostly of perimeter blocks arranged in a grid, with a diagonal route between the southeast corner of the Wheat Quarter site and the former factory buildings. The subsequent Consented Scheme adopted and developed the layout on the MTVH Site, but presented a different layout on the North: two perimeter blocks were placed to the east of the factory buildings (instead of a single perimeter block that included the factory buildings).

One of the key objectives set out in Chapter 1 of the BRW SPD is to “create urban grain capable of supporting appropriate uses while reflecting the town centre morphology”. The document further indicates that:

6.12 The proposed urban grain of the Wheat Quarter site should be characterised by large blocks, continuous building lines and active frontages. Wide streets provide an opportunity for surveillance and tree planting which will play a crucial role in security, micro-climate and street enclosure.

6.13 Perimeter block forms should predominate and should be based on the layout shown [...]. The definition between private and public place should be clear, with frequent activity along front facades supporting a good level of animation on to the street.

6.14 The existing scale of the built form on the Wheat Quarter site provides a context for the block sizes and scale to

be achieved through redevelopment.

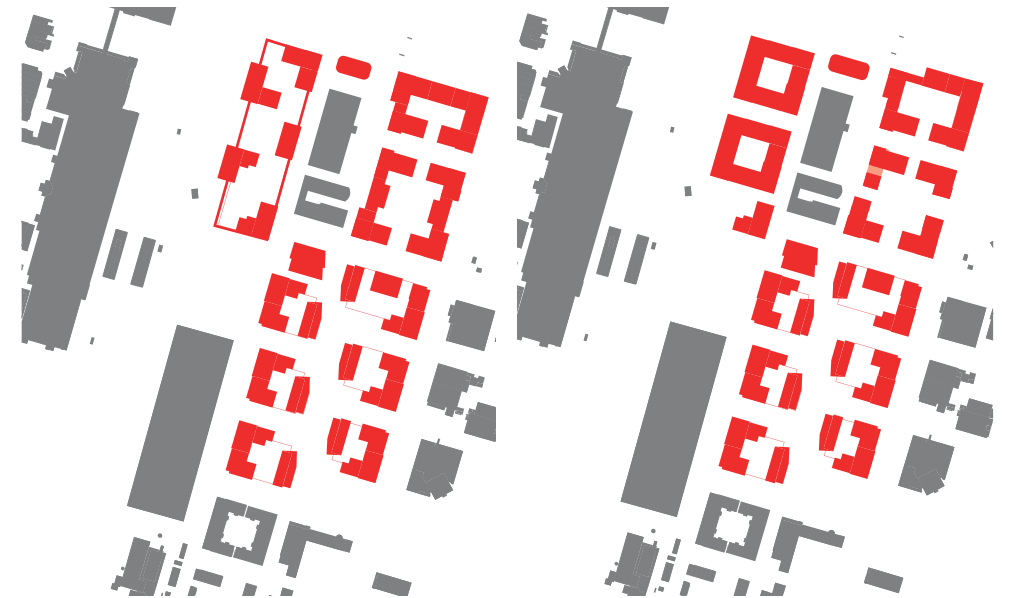
New development should not, however, simply replicate existing building mass but relate to those buildings being retained and the wider surrounding area.

The proposed scheme builds upon the principles established in the Masterplan Framework and developed for the consented schemes. Furthermore it also fulfils a new development brief evolved for specific end users whilst seeking to improve some aspects of the consented design.



Masterplan Framework, from the Broadwater Road West Supplementary Planning Document (adopted 2008)

Consented scheme (2015)



Consented scheme (2019)

Proposed scheme



4.5. CHARACTER AND TYPOLOGIES

The North and MTVH Sites are defined not only by the footbridge-Hydeway route that cuts across the Wheat Quarter site. There is also a natural difference in character: the north site is dominated by the factory buildings and exposed to the railway and Bridge Road, whereas the MTVH Site is clear of any buildings, separated from the railway, and closer to the Peartree residential area.

The number and location of new connections (pathways and roads) define the urban grain. For the Wheat Quarter, those connections are determined by the disposition of the factory buildings.

On the north site, the size, visibility, and large footprints of the retained factory buildings, together with the exposure to pedestrian traffic (between the train station and Hydeway) and car traffic (Broadwater Road and Bridge Road) indicates the potential for mixed use and vibrant civic spaces. A new beating heart for both sites is proposed around the Listed buildings which benefit from the existing and refurbished pedestrian bridge that connects Goodman Square and Hydeway into the Town centre on the west side of the tracks.

The existing industrial aesthetic inspires hard surfaces/pavements, an urban grain commensurate with the factory buildings, and a strict arrangement of building forms. These will be defining character elements on the north site.

Within the north site, there is a contextual differentiation between

the areas to the east and west of the factory buildings. The area to the east faces Broadwater Road (which is a primary road) and is divided into two blocks by the new link road. These are enclosed perimeter blocks with continuous frontages all around, except along link roads, where breaks in the buildings provide gateways into internal courtyards. Car parking is provided below ground.

The area to the west of the factory buildings faces the railway and is edged by the service road, off Bridge Road. Here, the building forms have been revisited from the Extant Consent to propose two new link roads in between three new urban blocks.

At the heart of the two primary urban blocks lies dedicated amenity space above a basement car park.

The most southern building, which generates the termination point of Hydeway on Goodman Square, has been modelled to reveal views of the Silos from the station exit on the footbridge into the town centre.

The two new link streets each lead up to the two new entrances to the Listed Buildings. Namely the entrance to the proposed Art Centre as well as the new entrance to the Production Hall on Reiss Walk.

The different character zones are further defined by differences in building uses, heights, dwelling types, residential tenure, and architectural design.



4.6. USE

The proposed land use strategy is derived from the Broadwater Road SPD.

Mixed Use

Mixed-use buildings are located primarily along the main routes: east-west (through Hydeway, Goodman Square, and the footbridge); north-south (the former factory buildings and the new Civic Building, which activate the pedestrian areas around them).

Residential Community

The regenerated Shredded Wheat factory site will be sustained by a new residential community. This new community must have sufficient size and density to inhabit the Wheat Quarter site and provide the occupation and movement necessary for mixed-use facilities of this scale.

Employment:

Most of the employment opportunities will be located within the converted former Production Hall and new Louis de Soissons building on Goodman Square.

Leisure opportunities:

The Art Centre is proposed within the former Boiler House and Grain silos. A gym and Art & Museum hub will be located in and around the former factory buildings.

Community facilities:

A cycle hub, gym and healthcare facilities are proposed within the Bridge Building which sits by the pedestrian bridge into the Town centre and adjacent to the Sensory Garden.



- Land use key
- Residential block
 - Re-use of listed structures
 - Leisure
 - Employment
 - Mixed use block

Above: Land use strategy plan (Figure 5.2) from the Broadwater Road West SPD

Opposite page: Proposed land use strategy plan



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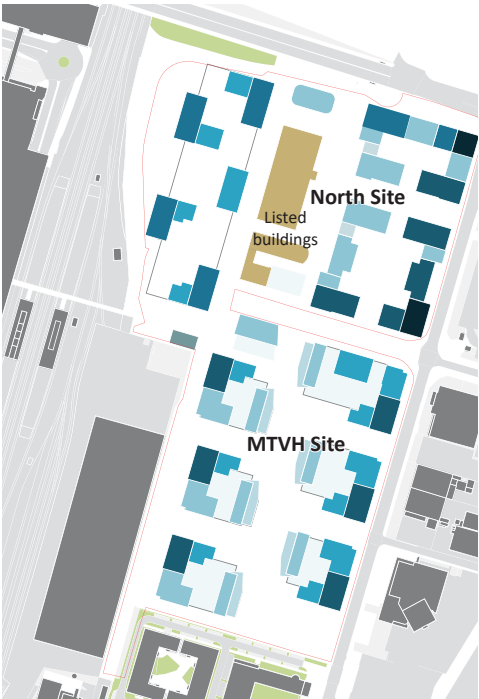
4.7. AMOUNT OF DEVELOPMENT

Given the prominent location, excellent public transport accessibility, unique site characteristics and the nature of the wider Development Strategy for the District, the scheme seeks to densify the residential development in certain areas of the Wheat Quarter site through a focus on diversifying the residential offer and the associated briefs against these occupier profiles.

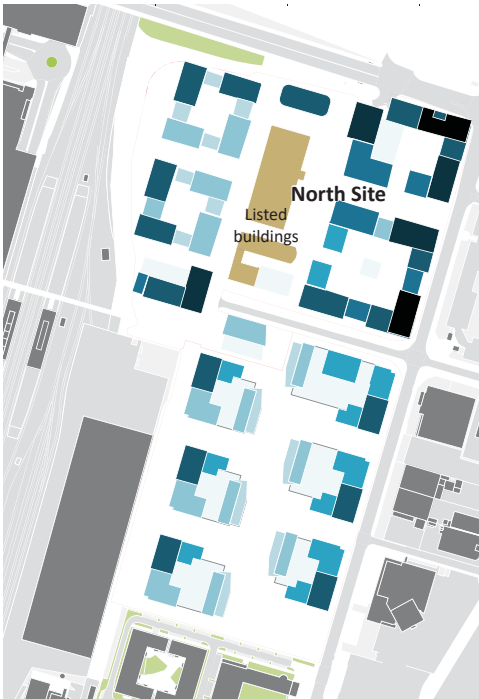
Component Part	2019 Consented	2020 Proposed	Difference	Notes
Class C3 Residential	697	560	-137	Reduction due to change in tenure (increase in residential care and introduction of PRS component)
Class C3 PRS	0	399	+399	Significant PRS component incorporated for tenure diversity and immediate delivery
Class C2 Residential Care Community	114	250	+136	Increase required to secure leading market operator and diversity of tenure to drive delivery
Non-Residential	12,700*	15,000	+2,300	Additional 1,131sq.m floorspace proposed through Community Bridge Building to incorporate Cycle Hub and Leisure/Business Uses Additional floorspace proposed in the form of small non-residential pods within Residential Blocks 1 and *1,200sq.m consented through 2019/1347/FULL for additions and improvements to Listed Building and 11,503 through 2018/0171/FUL
Parking	695	742	+47	600 residential and 142 non-residential spaces.

4.8. HEIGHTS & MASSING

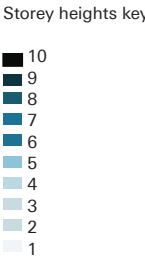
EXTANT CONSENT



PROPOSED



Building heights and their distribution in the consented scheme (above left) are similar to those of the proposed scheme (above right). However, the proposed buildings along the western and eastern edges of the Wheat Quarter site are, in general, are one storey taller than the corresponding buildings in the consented scheme.



4.9. ARCHITECTURAL DESIGN PRINCIPLES

BUILDING TYPES AND FORMS

The proposed buildings affirm a contemporary design informed by a rich context – Welwyn’s character as a planned Garden City, the former Shredded Wheat factory’s unique design legacy and historical significance, as well as the strategies put forward by the Broadwater Road West Supplementary Planning Document and principles established by the current Planning Consent.

A summary of the main design principles that picks up where the extant Consent scheme left off includes:

- building scale and mass appropriate to location;
- emphasis on the urban composition around the monumental industrial buildings;
- emphasis on key views into and across the Wheat Quarter site;
- adequate sunlight and daylight in all spaces and places;
- strong integration of buildings and landscape
- active street frontages with front doors to communal entrances;
- well connected car parks, refuse and cycle stores, with direct access from building cores;
- clusters of buildings with varied heights, allowing roof terraces and enlivening the townscape;
- a simple but imaginative palette of materials and building details inspired by the local context, with brick as the main facade material.
- architectural referential details to the Former Shredded Wheat factory



MATERIALS AND DETAILING

This proposal aims to provide enough variety and contrast of materials and architectural details across the large site, within the limits of feasibility and aesthetic consistency. Hence, a consistent palette with a limited range of materials will be used, with enough variation in tone and texture to express the desired identity to the various uses.

Materials will be chosen for their high quality, robustness, durability, and environmental sustainability.

Brick and metal, in a complementary range of tones and textures, will be the predominant facade material. Window and door frames, as well as balcony railings, will be made of metal, coated in colours complementary to the brick cladding and making reference where possible to the Wheat Quarter sites heritage.



RESIDENTIAL DESIGN

All dwellings will be flats. However, the latest proposals seek to diversify the residential offer by introducing PRS and a significant amount of later living to the Wheat Quarter site.

The proposed residential designs are based on the standards set by The Department for Communities and Local Government's Technical housing standards – nationally described space standard, in addition to the Design Team's experience and advice obtained through consultations. Some important design principles that inform this proposal are:

- well defined, generous communal entrances;
- active street frontages,
- clearly defined defensible spaces at ground level;
- large proportion of dwellings with multiple aspects;
- direct routes from cores to apartments;
- a balanced mix of dwelling sizes and housing types;
- generous private amenity space for all, with usable balconies sufficient for outside dining;
- easily accessible, centralised bin stores;
- secure car and cycle parking with direct access from building cores;
- home office space to promote flexible work and allow for working from home.

ACTIVE FRONTAGES

Active frontages are maximised throughout the scheme with the inclusion of both communal and private residential entrances at street level to all elevations.

Communal entrances will be clearly visible and transparent. Primary entry will be from public paths, but direct to secure car parks will also be provided. Direct routes will link the vertical circulation in each building to secure amenity areas. All ground floor dwellings facing public pathways will have usable defensible spaces.

The design of entrances and doorways across the Wheat Quarter site varies from building to building, adding variety and character along the public spaces.

Plant room and service access frontage will be kept to a minimum, but cycle stores will be adjacent to entrance lobbies or close to residential cores and benefit from street frontage where possible, to facilitate security and easy access.

PRIVATE AMENITY

All dwellings will include private outdoor amenity space in a variety of types: front gardens, recessed and projecting balconies, and roof terraces.

Recessed balconies will generally be located along the public thoroughfares on the outer edges of the Wheat Quarter site, whereas projecting balconies will generally overlook amenity areas within the Wheat Quarter site. Balcony dimensions will allow meaningful use – e.g. outdoor dining.

Most dwellings facing communal gardens or courtyards will benefit from private gardens with access to the communal gardens. Where private amenity space faces directly onto a street, it will be well defined defensible space.

Some buildings will be split into elements with different heights, thereby providing variety in the townscape as well as roof terraces which will add to the amenity provision.

SUNLIGHT & DAYLIGHT

The application site is bound by mainline railway line on the west and industrial buildings on the north and east orientations. The taller building elements are proposed along the outer edges of the Wheat Quarter site, so as to minimise overshadowing of the amenities as well as impact on the Listed Buildings.

The masterplan has been developed to create a comfortable microclimate and usable outdoor spaces / public realm. The proposed buildings will have minimal or no impact on buildings in adjacent sites.

Building blocks have been arranged so as to avoid overshadowing and provide accessible amenity spaces. Opposing facades have been kept at a minimum separation distance of 20 metres. A large percentage of the dwellings have dual aspect living rooms to ensure adequate daylighting. Throughout the design process, building volumes and landscape have been adjusted – within the parameters of the project brief – to optimise the usability of outdoor spaces such as play areas.

Thus, the architectural design complements the landscape and public realm design, with the aim of providing enjoyable outdoor amenity that can be used as extensions of the living spaces, safely and comfortably at different times of the day and throughout the year.

4.10. DESIGN PRECEDENTS

WELWYN GARDEN CITY RESIDENTIAL ARCHITECTURE



Most residential buildings in the town centre date from the 1920s and 1930s. In accordance with Louis de Soissons' masterplan, they were designed in a neo-Georgian style, with brick facades and white-painted sash windows. Most dwellings are detached cottage-like houses lined along wide leafy streets.

WELWYN GARDEN CITY PUBLIC BUILDINGS



C.H.Elsom. Council Offices, Welwyn Garden City. Completed 1935.



Richard Sheppard Robson & Partners. Campus West, Welwyn Garden City. Completed 1975.



The Howard Centre. Completed 1990.



Penoyre & Prasad. Queen Elizabeth II Hospital, Welwyn Garden City. Completed 2016.

Several public buildings have been built since the city's inception. Changing needs and forms of expression have produced a range of architectural designs in styles that span revivalism, modernism, post-modernism, and contemporary architecture.

INDUSTRIAL ARCHITECTURE



The original Shredded Wheat Factory buildings, as designed by Louis de Soissons. The Production Hall's main elevation (as seen above) has not been visible since other buildings were appended.

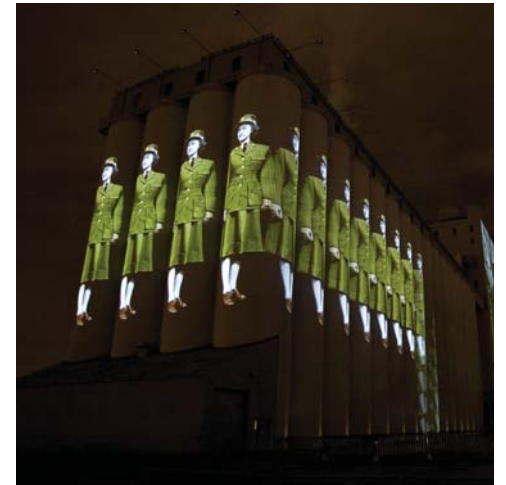


The former Bankside Power Station in London was converted into an art gallery (The Tate Modern, project by Herzog & de Meuron, completed 2000). The main internal space is connected to the outside plaza through a ramp. The new art museum has been instrumental in regenerating a large disused industrial building, as well as a prominent London site.



Many converted industrial buildings have been playing an important role in urban regeneration in cities around the world.

PUBLIC ART



From Antwerp to Bilbao, Cape Town, London, and countless other locations, public art and land art have increasingly been playing a crucial role in reinvigorating public places around the world. Disused industrial sites, buildings and infrastructure offer particularly relevant opportunities for the use of public art in urban regeneration.

RESIDENTIAL ARCHITECTURE



Hackney Wick, East London



RESIDENTIAL BUILDING DETAILS



KjellanderSjöberg. Maja Gräddnos residential block. Stockholm, Sweden. Completed 2015.



BaumschlagerEberleArchitekten. Verwalterbuildings. Dornbirn, Austria. Completed 2013.



Like the consented scheme before it, the proposed scheme takes inspiration from a number of contemporary designs – where the context is comparable and the thoughtful detailing provides remarkable quality and delight.

4.11. LANDSCAPE DESIGN VISION

VISION

The Broadwater Road site at Welwyn Garden City offers an opportunity to provide a high quality, mixed use scheme creating a vibrant new community at the heart of Welwyn Garden City, helping to redefine the former Shredded Wheat Factory site and provide much needed housing, community facilities and additional open space.

In order to build on the principles and on many of the design details established in the consented scheme, Bradley Murphy Design have been retained as landscape designers for this application.

The landscape proposals continue to be underpinned by a coherent vision that draws upon the principles and qualities of the existing garden city as well as the Wheat Quarter site's location, heritage, character and the wider setting of Welwyn Garden City. This vision formed the basis of the consented scheme and has been carried through to inform the current landscape masterplan.

The vision has four main strands:

Heritage

- Retained 1920's listed buildings will provide a special heritage focus to the development.
- The Wheat Quarter site features the landscape character zone of the industrial look and feel of the former Shredded Wheat factory on the North Site.

Beautiful Spaces

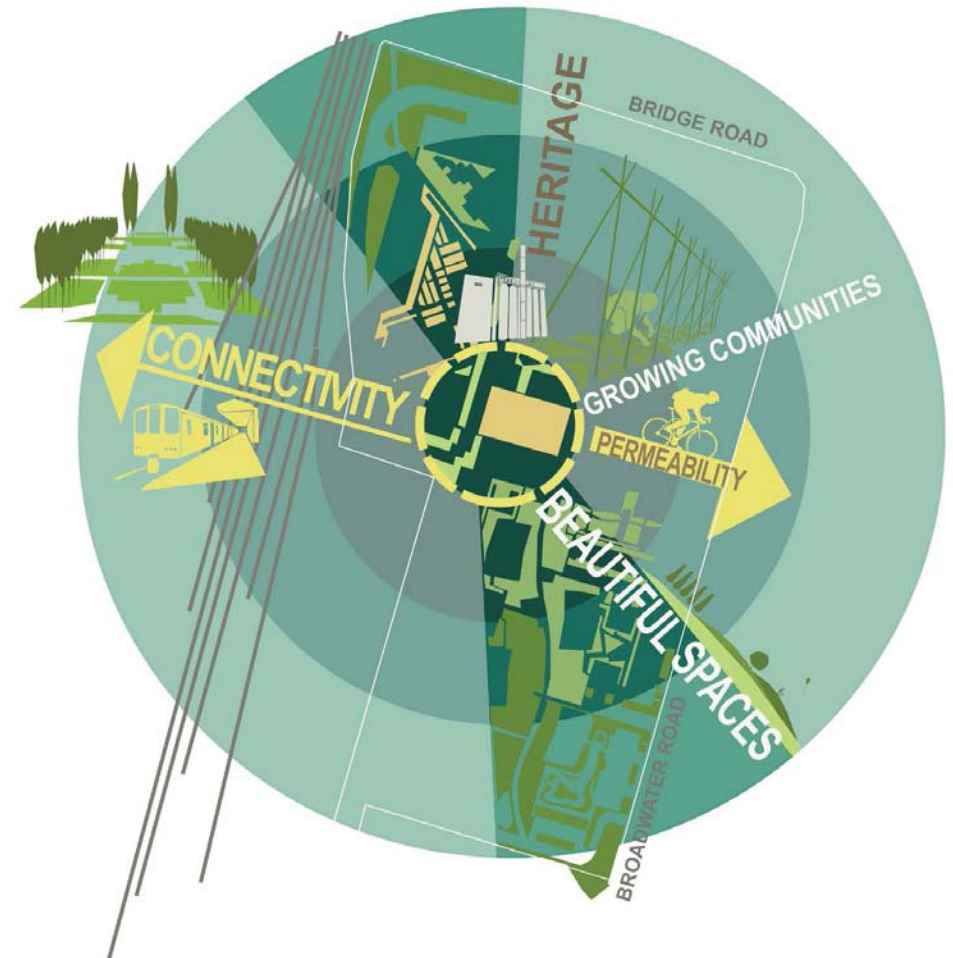
- Landscape and open space integral to characterising the development
- Creation of public open spaces offering green routes in to the development
- An urban square at the heart of the development creating a new social focal point in the town

Sustainable Communities

- Fostering a sense of community to allow residents to integrate and support one another to create an inclusive mixed use community
- Introduction of a Sensory Garden in consultation with the Gardening for Disabled Trust, in place of the previously proposed Allotments

Connectivity

- Reinforcement of connectivity across the railway and in to the town
- Forging of new links to Peartree ward and the wider footpath network in the east



4.12. LANDSCAPE DESIGN PRINCIPLES

In this setting of Welwyn Garden City, the quality and contribution of the landscape to a development is fundamental to its success in terms of placemaking. With a new building layout, the underlying garden city principles that drove the design process on the extant consented scheme has been crucial in underpinning the masterplanning exercise that has taken place to support this fresh planning application for the Wheat Quarter.

The landscape strategy has been developed as part of an iterative masterplanning process, ensuring that the landscape and setting characterises the development just as much as the built form. The design has continued to evolve to respond to a new set of needs and aspirations, fostering a sense of ownership from both potential residents and those within the wider community.

The Wheat Quarter site's location, in an area defined by a wide range of ongoing development, offers a unique opportunity to improve and forge new links between Peartree in the east and Handside in the west. The design and layout of the landscape and public realm has carefully considered the movement of pedestrians, cyclists, potential residents and visitors. The resultant series of interconnected flexible green spaces respond to use and movement and help to stitch the new development in to the existing urban fabric.

The heritage context of the Wheat Quarter site and the importance of the 1920's silos and production hall are still key features of the new Wheat Quarter masterplan. These buildings will create a cultural focal point to the wider scheme, helping to characterise streets, frame views and provide a setting for a variety of both formal and more industrial style spaces.

Soft landscape combines a range of planting typologies to create a multifunctional habitat mosaic. Planting is introduced in the form of vertical landscape and through the use of green and brown roofs, creating functional and usable green space at both upper and lower levels. The sensory garden offers a destination space for a variety of users and creates a place to explore, learn about and experience planting with scent, colour and texture at the heart of the development.

The scheme proposes a range of both informal and more formally laid out areas of public realm. Reiss Walk utilises the original rail lines serving the grain silos and production hall to create a vibrant space with an industrial character. Louis de Soissons' Gardens feature more formally laid out areas of lawn, replicating the original recreation space provided for workers in the 1920's. Goodman Square sits at a pivotal location, creating an important gathering space for visitors and pedestrians bordered by shops, bars and restaurants.



4.13. ENERGY & SUSTAINABILITY

ENERGY STRATEGY

This report has been compiled in order to provide an Energy Demand & Carbon Dioxide (CO2) Emissions Assessment for the planning application for the Wheat Quarter, a proposed mixed used development on the northern part of the Wheat Quarter site of the former Shredded Wheat Factory in Welwyn Garden City.

This document has been compiled by Sol Environment Ltd on behalf of Plutus Estates (WGC) Limited ('the applicant'). The Energy Demand Assessment has been formulated to provide a sustainable solution for the proposed site in accordance with Welwyn Hatfield Borough Council Local Plan. This assessment will be specifically focused on Policy SADM 13: Sustainability Requirements and Broadwater Road West Supplementary Planning Document Site Indicators.

The energy assessment and subsequent strategy has been prepared such that it is aligned with the Energy Hierarchy (see Section 2.1), with focus on sustainable building design (reduction of energy consumption at source), provision of energy efficiency measures and installation of building-integrated LZC technologies.

The strategy confirms how the development achieves compliance with current energy planning policy. Broadwater Road West Supplementary Planning Document Section 7 Core National Indicator 2 requires all new developments in the Broadwater Road West development area, achieve a site target of at least 10% of their energy requirements from decentralised and renewable or low-carbon sources. Policy SADM 13 requests that all major development proposals must demonstrate that they have sought to maximise opportunities for renewable and low carbon sources of energy supply where consistent with other Local Plan policies.

To meet building regulations and achieve the planning policy requirement of >10% saving through the use of renewable or low carbon technologies we propose the following. The recommended energy strategy is based on a fabric first approach, utilising passive design measures, well insulated and airtight building fabric, MVHR and space and water heating provided by a communal heat pump system for each block. A different heating system is proposed for Block 7 only this will include an all-electric scheme with panel heaters and HWS cylinders with the >10% saving achieved through the installation of a roof mounted PV array.

Below is a summary of the measures recommended to ensure the local authority's requirements will be met:

- High performance super insulated building fabric u-values
 - o External Walls – 0.16 W/m2K
 - o Floors – 0.11 W/m2K
 - o Roof – 0.11 W/m2K
 - o Door & Windows – 1. 1 W/m2K
 - o Party Walls – 0.0 W/m2K
- Air Tightness – <3.0 m3/(h.m2)
- Thermal Bridging y value – 0.05 W/ m2K (Accredited Construction Details assumed)

- Space and Water heating (all except Block 7) – a roof mounted heat pump system for each block will support a low temperature primary energy loop. A water to water heat pump within each unit will tap into the energy loop boosting the temperature for space and water heating. Space heating will be delivered by radiators and underfloor heating and controlled via time and temperature zone control.
- Space and Water heating (Block 7) – all-electric scheme with panel heaters and HWS cylinders.
- LZC Technology – the 10% reduction in energy use will be achieved through the installation of a roof mounted PV array for Block 7 and the use of heat pumps in the remaining blocks.

Former Shredded Wheat Factory, Welwyn Garden City: Energy Strategy Summary			
Scenario	Regulated Energy Use (kWh / year)	Regulated CO2 Emissions (kgCO2 / year)	Ave BER/DER (kgCO2 / m2 / year)
Baseline Scenario without On-Site LZC Technologies	18,329,862	2,739,814	31.70 (South) 17.27 (North) 43.8 (Non-Resi)
LZC Scenario with On-Site LZC Tech (PV and CHP)	15,864,667	2,171,766	26.97 (South) 11.03 (North) 39.5 (Non-Resi)
Total Saving achieved through the Implementation of on-site LZC Technologies	2,465,194	568,048	-
Percentage reduction	>10%	>10%	>10%

SUSTAINABILITY STRATEGY

Welwyn Hatfield Borough Council is committed to achieving sustainable development. As a result, the current planning policies for this area emphasise the importance of sustainable growth with particular emphasis on the encouragement of energy efficiency and green energy and industry.

It is important, therefore, that the proposed development site contributes to the Council's sustainability aims as well as meeting regional and national objectives for sustainable development.

This sustainability statement demonstrates that the proposals satisfy a number of key objectives, responding to local needs and

requirements and conforming to current good practice.

In addition, this strategy confirms that the development will meet the Broadwater Road West Supplementary Planning Document target to reduction in carbon dioxide requirement of >10% through the installation of low carbon and renewable technologies, and in accordance with Policy SADM13 the major non-domestic elements of the development will be assessed against the BREEAM methodology targeting a rating of Excellent (or Very Good if Excellent is demonstrated to be not technically feasible or viable).

In summary, the sustainability statement has informed the design process by identifying opportunities and constraints for sustainable development, and the process has highlighted the proposals sustainability performance against national, regional and local planning policy.



Some of the design measures which will underpin the sustainability of the proposed development: encouraging pedestrian and cycle movement; managing surface water on the ground and roofs; promoting the use of public transport; specifying sustainable materials; providing a variety of outdoor spaces to be enjoyed by the entire community; promoting biodiversity.

THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

5. ACCESS

5.1. SUMMARY OF ACCESS AND MOVEMENT STRATEGY

ACCESSIBILITY PRINCIPLES

This development proposal aims to ensure that all users will have equal and convenient access to buildings and spaces.

This is achieved by adopting the overarching principles of inclusive design set out in the National Planning Policy Framework and in the Welwyn Hatfield District Plan, in particular the District Wide Policy D9: Access and Design for People with Disabilities, allowing access for disabled people, children in prams and pushchairs and those who are temporarily disabled through accident or injury.

The development has been designed to comply with national standards and guidance including Part M of the Building Regulations and BS 8300.

This proposal includes the following inclusive design measures:

- All approach routes on ground level will be via wide paved paths and the entrances will be clearly legible on the frontage of each building.
- Up to 10% of the parking bays will be designated and marked as accessible.
- Designated cycle routes will be provided throughout the Wheat Quarter site; secure bicycle storage facilities will be located at ground level.
- Mixed use buildings will have separate entrances for residential, commercial and public use.
- Each building will have level entrances.
- Entrances will have swing doors. Where revolving doors are used a swing door will also be provided, and where necessary power operation will be used to overcome the 30N door pressure requirement.
- Transitional lighting levels outside building entrances and foyers shall be similar.
- Manifestation will be used for glass screens and doors.
- Floor surfaces will be slip resistant with large wipe-off areas immediately inside the main entrances.
- Lift design and capacities will follow recommendations from the lift consultants and will meet best practice standards.
- Accessible and ambulant WCs will be provided at all levels of non-domestic buildings.
- Lifts are used throughout the scheme complying with Part M requirements and will have a clear space of 1.5m in front of their entrances.
- The heights of landing and lift controls will be Part M compliant.
- Residential bathrooms are designed to Life time Homes standards, allowing for future adaptations such as handrails and hoists.
- The layout of the bathrooms will allow for ease of access to the bath, WC and wash basin.
- Switches, sockets, ventilation and service controls in

residential units will be located at Part M compliant heights.

- The C2 units proposed have additional features to meet the specific needs of the intended users. These include wheelchair turning space within each room and wider internal doors.

RELEVANT LEGISLATION AND DESIGN GUIDANCE

- Equality Act 2010;
- National Planning Policies Framework and National Planning Practice Guidance;
- Saved policies of the Welwyn Hatfield District Plan adopted 2005 and supplementary planning guidance;
- Broadwater Road Supplementary Planning Document (BWR SPD);
- Welwyn and Hatfield District Plan Review - Supplementary Planning Guidance, Car Parking Standards (Adopted January 2004);
- Part M of Approved Building Control Documents;
- BS 8300;
- London Housing Design Guide.

5.2. PEDESTRIANS

The masterplan provides a network of streets and publicly accessible spaces designed to be legible and to allow easy access. New connections have been created, and the accessibility of existing routes has been improved.

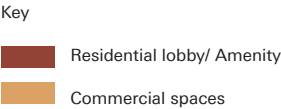
Landscaping of publicly accessible spaces has been designed to provide inclusive access throughout. Pedestrian crossing points along new roads and streets will either provide level access or dropped kerbs. External ramps will be designed to guidelines as set out in Approved Document Part M of the Building Regulations.

Publicly accessible car parking will provide for at least the recommended amount of accessible car parking standards as outlined in Welwyn and Hatfield District Plan Review - Supplementary Planning Guidance, Car Parking Standards (Adopted January 2004).

The design and access arrangements of the individual blocks, described in later sections of this report, are designed to be fully compliant with guidelines as set out in Approved Document Part M of the Building Regulations.

Railway Footbridge, Lift, and Stairs
As part of the enhancement of the railway footbridge and in order to create a more dramatic approach to the Wheat Quarter site, the existing public stairs will be replaced by new, metal framed stair to match the design of the old footbridge, which will create an enhanced arrival for pedestrians entering this exciting new Quarter.

To provide DDA compliant access to the footbridge an external lift will be placed next to the new stairs. The lift shaft will be clad in dark glass and black metal framing, to match the contemporary design of the Louis de Soissons Building.



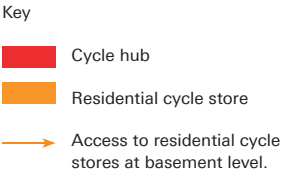
5.3. CYCLISTS

On-street cycle parking will be provided in the main public spaces around Goodman Square and the railway footbridge. There is also a new cycle hub proposed adjacent to the footbridge.

Safe and secure private cycle storage will be provided within each residential block as shown on the diagram below. Residents' cycle stores will generally be located adjacent to entrance lobbies and, as much as possible, with direct access to the peripheral road network and to secure car parking.

Cycle Hub
The new facilities will be delivered by the Wheat Quarter development which offers a unique opportunity in Welwyn Garden City to introduce infrastructure, information and incentives to promote walking and cycling as a first choice of transport. The proposed development has sufficient mass to act as a positive catalyst for change and to influence travel behaviour.

More information is available within the 'Wheat Quarter Cycle Hub' brochure submitted as part of this application.



5.4. ROAD VEHICLES

Most access points for cars are located on Broadwater Road, with an additional access point on Bridge Road – as illustrated in the diagram on the opposite page.

Car movements have been limited to the periphery of the Wheat Quarter site to ensure that as much as possible of the areas used by pedestrians are free from car traffic.

A designated taxi pick up and drop off point and queuing facility for 5 taxis has been located on Hydeway as illustrated on the diagram opposite. A turning circle for all vehicles entering Hydeway has been located at the western end of the road, adjacent to Goodman Square. Car and taxi access into the square will be prevented by means of a set of drop down bollards, which will only afford access to refuse, emergency and delivery vehicles as described below.

Car parking is largely located within undercroft or basement areas but is supplemented by a small amount of on-street parking. Vehicular access beyond the on-street car parking

spaces will be prohibited through the landscaping design to ensure vehicles are not able to pass into the defined publicly accessible spaces.

Provision of wheelchair accessible car parking spaces has been dictated by the following statutory and other relevant guidance documents:
Welwyn and Hatfield District Plan Review - Supplementary Planning Guidance, Car Parking Standards (Adopted January 2004)
The London Housing Design Guide

In accordance with the above documents, accessible car parking spaces will be provided as follows:
Residential - 10% of total unit numbers (W&HDPR) or 1 space for every dwelling built to mobility standards (LHDG). Note; we are proposing 10% of dwellings are built to mobility standards;
Employment generating development - 5% of total capacity
Other premises to which the public have access to (i.e. A1, A3 and A4 units, C1 hotels and D class uses - 6% of total capacity.

Key

Secondary road

Tertiary road

Covered parking

Basement parking



5.5. DELIVERIES & REFUSE

It is proposed that a centralised management system will be implemented for the collection of both domestic and commercial refuse and recycling. Collection points have been indicated on the diagram opposite where refuse and recycling bins can be towed to by a refuse operative on collection day, disposed of and the bins towed back to designated bin areas located within the undercroft areas of the individual blocks. As with car movements, it is proposed that refuse and recycling lorry movements are limited to the periphery of the Wheat Quarter site, with no requirement for vehicles to track across defined public accessible spaces - again illustrated on the diagram opposite.

Delivery vehicles serving the non-residential areas of the Wheat Quarter site will enter via two access points – one off Bridge Road and the other off Hydeway. The proposed workspace and crèche areas in Block 4, healthcare facility in Block 3 and the A3/A4 units within Block 2 will all be served via the road running along the northern and western boundaries of the Wheat Quarter site, joining Bridge Road at the existing junction point.

Delivery vehicles serving Block 4 will use the lay-by area to the north of the building, which will also provide a drop-off and pick-up point for the crèche, healthcare facility and

workspace.
Class E units located within Block 2 as well as the adjacent community building will be served by a designated service bay located to the south of Block 2 as illustrated on diagram opposite.

Deliveries and collections to non-residential areas in blocks 1, 4, 7, and 8 will all be circulate via Hydeway.

Delivery vehicles will be able to gain access to defined service set down areas, located on the eastern edge of Goodman Square, via a set of drop down bollards. The purpose of the drop down bollards is to stop cars and taxis entering the square and will be controlled by the central management office.

More detailed access arrangements for the individual blocks are described in the Transport Assessment under separate cover.

Key

Refuse store

Delivery Point



5.6. SECURE BY DESIGN

Integrated Approach

The scheme has been designed with safety and security in mind in order to create an safe place to live, work and visit.

Activation

Redevelopment of this disused brownfield site will create a new destination at the heart of the city. The Wheat Quarter site will be activated by the new residents, on site employees, visitors of the new art centre as well as people passing through to access the neighbouring station.

The proposed residential offer provides a mix of accommodation types. Alongside the traditional flats for sale, there are private rented apartments (block 7) and the elderly living building (block 6). The variation within the accommodation types on offer will bring a mix of people into the Wheat Quarter site ensuring it is activated throughout the day.

Both the PRS and C2 buildings contain significant amenity spaces. The use of these spaces throughout the day and evenings work to activate these parts of the Wheat Quarter site and improve passive surveillance. The design of both of these blocks allows for concierge space which adds to the sense of security. Due to the extent of the amenity spaces proposed their is likely to be a site manager and other operational staff required throughout the day.

There are also a number of commercial elements within the proposal which provide further activation. These help to ensure that the Wheat Quarter site is active throughout the day.

Public realm

High quality public realm has been proposed along the key route from the station to the east which will help to further activate the Wheat Quarter site. The geometry of the buildings and design of the landscape allows for long views throughout the Wheat Quarter site increasing natural surveillance.

Lighting

Lighting throughout the development is important to reduce both crime and fear of crime. The proposed residential entrances will be lit and there will be even white light throughout the proposed public realm.

Principles

The proposed design aims to provide safe and secure spaces, and is informed by Official Police Security Initiative’s Secured By Design (SBD) principles such as:

- ground floor properties have defensible edges defined by low walls and hedges maintaining good visibility of entrances;
- all streets and spaces are over-looked with windows, to promote natural surveillance;
- no blank gable walls;
- all communal entrance doors will incorporate an access control system via an external door entry panel with video/intercom control from individual dwellings;
- access to communal courtyards will be secure with access control for residents;
- car parks and bike stores are all accessed via secure gates, shutters and doors, incorporating CCTV coverage;
- all glazing systems, locks and doors will be specified to SBD and BS standards;
- internal post boxes in communal lobbies will be surveilled by CCTV;
- lifts and staircases with access control and CCTV cameras;
- bin stores with access control are located close to cores.



CCTV Cameras



Example C2 ground floor amenities



Residential Courtyard



Vistas and active street

THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

6. PROPOSED DESIGN

6.1. PROPOSED MASTERPLAN

SCHEME OVERVIEW

Overall, the masterplan arrangement aims to provide legible spaces and enhance the connectivity throughout the Wheat Quarter site, while supporting a sense of community.

Based on the grid arrangement discussed previously the Site Layout forms on the North Site consist of three types: courtyard blocks and L-shaped blocks. Each of these forms is deployed in response to specific conditions on each location – such as the amount of space available, the arrangement of public spaces and landscape, distant views to landmarks on the Wheat Quarter site, etc.

The Wheat Quarter site can broadly be split into two parts with Hyde Way and Goodman Square providing the east-west spine where these meet.

The character that defines the north part of the Wheat Quarter site is defined by the post-industrial nature of the former Shredded Wheat Factory, which the proposed scheme seeks to respond to in both layout, public realm and landscape terms.

The images on the following two pages are aerial views of the overall scheme proposed compared with that of the Extant Consent.



6.2. MASTERPLAN OVERVIEW

EXTANT CONSENT



PROPOSED



EXTANT CONSENT

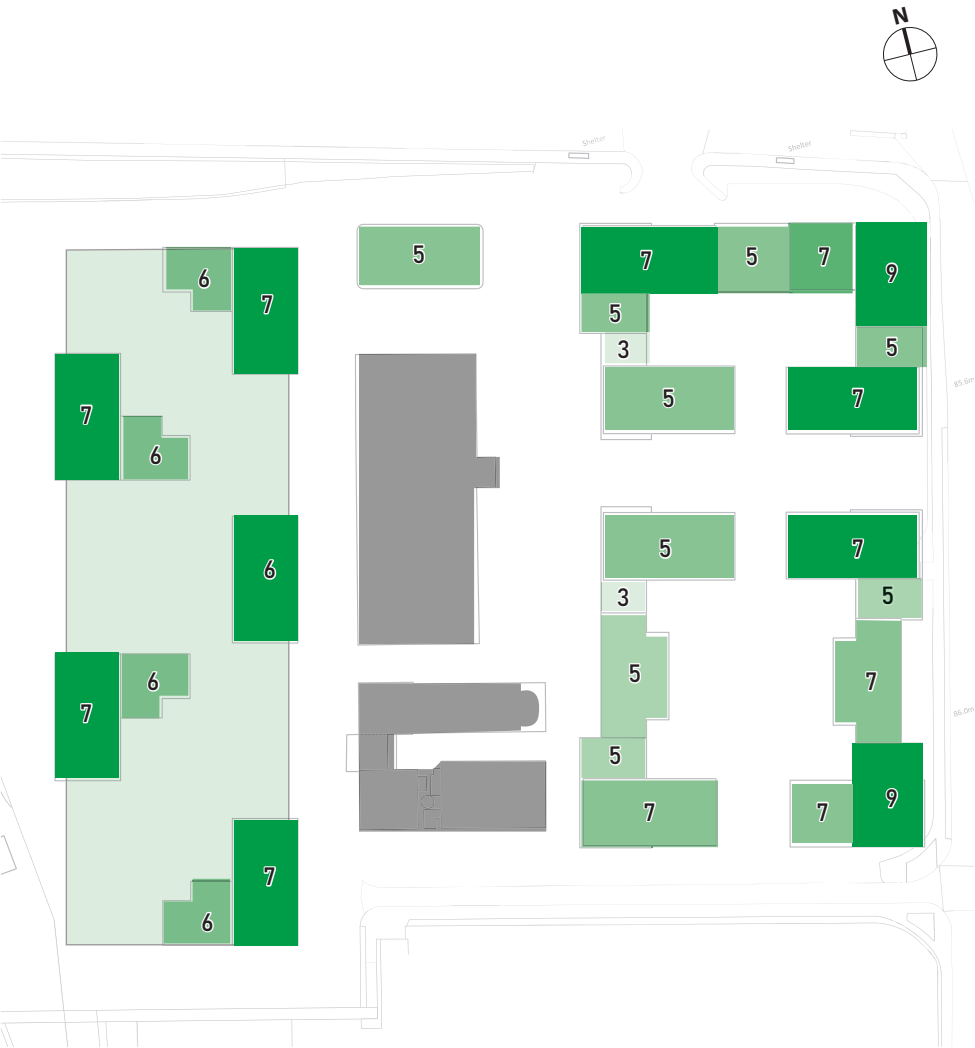


PROPOSED



6.3. BUILDING HEIGHTS COMPARISON PLAN

EXTANT CONSENT



PROPOSED



6.4. PROPOSED RESIDENTIAL LAYOUT

NORTH SITE

The North site can broadly be split into three character zones with the former Shredded Wheat buildings and structures acting as the central spine and heart to the proposal. Forming the southern elevation to Goodman Square is the mixed use LDS building. At the northern end against Bridge Road we are proposing a relatively small residential building to replace the warehouse buildings which once engulfed the original Shredded Wheat Factory buildings.

East of the Production hall, and on axis with the reinstated front door to this, we are proposing two perimeter blocks which seek to retain direct views of the Production hall from both Bridge and Broadwater Road whilst reinstating an urban edge against these thoroughfares.

The resultant urban blocks generate semi-private residential amenity spaces at their centre which afford the best possible aspect to the proposed residential units surrounding these.

To the west Blocks 1,2&3 are bound by the raised pedestrian bridge to the south, banked Bridge Road to the north and the railway lines to the west.

The proposed layout is a departure from the extant consent in so far as it seeks to remove the large podium parking and generate two new streets that connect residents to Reiss walk and the central spine of activity. By breaking the urban block we are able to retain views of the factory buildings from the rail tracks and west side of the Town centre.

Typically, residential blocks have central corridors linking flats on either side. The vertical circulation cores (typically with two lifts and fire stairs) are located centrally within each block, mostly in the locations least suitable for habitable spaces.

The use of simple layouts and repetition of a relatively small number of different dwelling types ensures the viability of the scheme. On the other hand, the envelope design provides variation and contrast through the use of different materials, fenestration patterns, and façade details.

The adjacent Typical Upper floor plan 50% of the units benefit from dual aspect, 38% are single aspect 12% are single aspect north facing units

In addition to the generous new public spaces and shared amenity spaces, which are described in detail within the Landscape Design Statement we are proposing to provide each residential unit with generous private amenity spaces in the form of gardens, roof terraces, external and inset balconies.

Gardens vary in size depending on the unit size and type from 5-20m²
Balconies vary in size depending on the unit type from 5-12m²
Roof terraces vary in size depending on the unit size and type from 5-25m²

KEY:

Dual Aspect

Single Aspect

Single Aspect North

Private Amenity space



THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

7. DESIGN
LOUIS DE SOISSONS

7.1. CONCEPT

OVERVIEW

Sitting at the eastern end of the footbridge, the Louis de Soissons building is one of the first buildings seen by pedestrians approaching the new residential and Cultural Quarter from the town centre and the train station. Accordingly, the proposed building has a special significance in the master plan. It accommodates a mixture of uses and sits on the most prominent location – Goodman Square – opposite the former factory buildings designed by the late Louis de Soissons.

Louis de Soissons Architects – the practice that carries on the legacy of Welwyn Garden City’s original chief architect – had been appointed to design the Civic Building which forms part of the extant consent.

ColladoCollins have since updated this design to accommodate an additional floor of accommodation within the previously consented envelope.

The proposed design for the LDS Building aims to provide an elegant, contemporary and suitably powerful entrance to the exciting and vibrant new Quarter. The distinctive form and materials contrast with the adjacent buildings – both existing and proposed – and are intended to complement the building’s setting.

The Louis de Soissons building has been set out in accordance with the current BCO (British Council for Offices) ‘Guide to Specification’.

Internal floor to ceiling height will be a minimum of 2.7m to main office areas (no ceilings provided within main floorplates – exposed structural soffits). 2.4m minimum to toilet core areas.

The LDS Building is proposed to be 6 storeys in height.

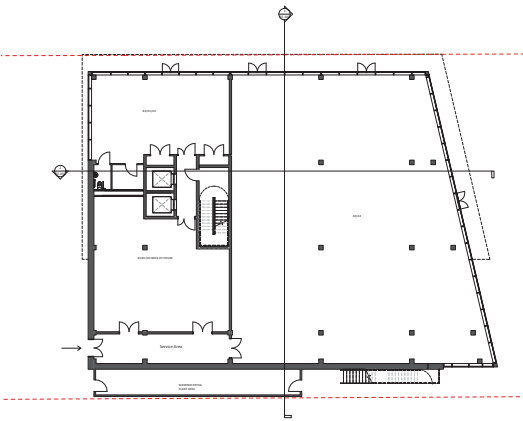


The LDS location diagram

USE, LAYOUT, ACCESS AND CIRCULATION

Gross internal area (GIA)	3,256m ²
Number of storeys	6

Floor	Use	GIA (m ²)
Ground	E/F1	745
First	E/F1	526
Second	E/F1	526
Third	E/F1	526
Fourth	E/F1	526
Fifth	E/F1	406

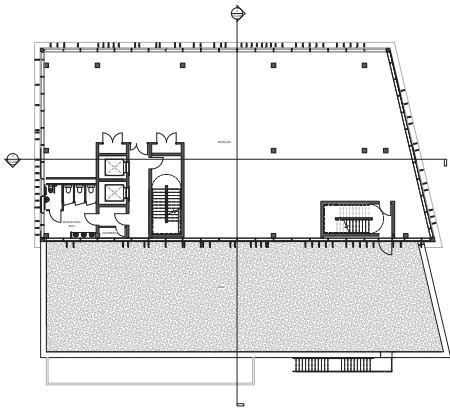


Ground floor plan

The ground floor unit is intended to be a restaurant or coffee shop for residents and visitors to the Cultural Quarter.

The First floor benefits from access to a south facing roof terrace. This terrace is proposed as communal space accessible by all the upper floors of the building.

Service access to the ground floor A3 unit and offices is discreetly located at the rear of the building, for deliveries and rubbish collection via Hydeway.



First floor plan



7.2. APPEARANCE

DESIGN DETAILS & MATERIALS

Initial sketches for this building were aimed at creating a gridded, white framed building, sympathetic to the character and form of the retained Listed Buildings.

That architectural language, however, was used for many of the proposed residential blocks elsewhere in the proposed scheme. It became clear that this prominent building needed to have a singular design, deliberately contrasting with the Shredded Wheat factory and the surrounding new blocks, and reflecting its particular use – neither residential nor cultural.

The building is designed as an elegant and simple dark glass block, clad with an irregular grid of projecting black metal fins that provide depth and a changing appearance when seen from around the Wheat Quarter site.

The ground floor is set back some two metres from the upper floors, so that the main bulk of the building appears to float above the ground. The overhang also provides shelter and shade to the ground floor unit.

The top floor is also set back some two metres, partly to create an attractive wrap-around roof terrace to the offices, but also to reduce the apparent bulk of the building.

The dark glass, minimal framing, and the black metal fins are elements of a contemporary design with an industrial legacy, in response to the previous use of the Wheat Quarter site and to the black metal framework of the important pedestrian railway bridge. Also, the original industrial building on this location was a tall, thin and unusually dark building, with deep bands of dark grey brickwork, so the design pays subtle homage to that building.

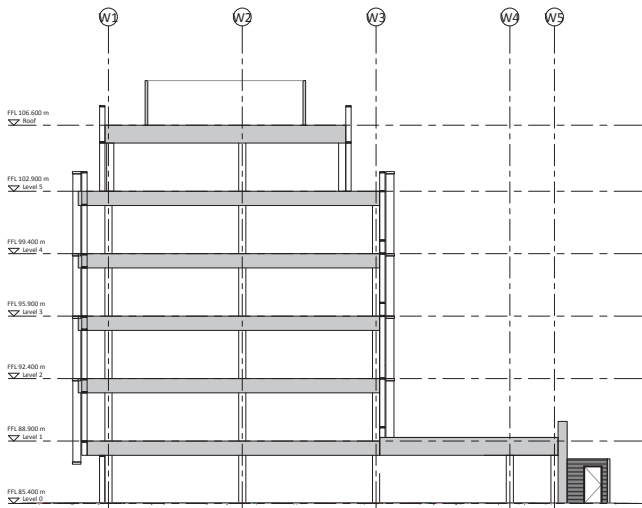
The projecting metal fins provide vertical emphasis to the elevations, which reflects the verticality of the



Design precedents. Left: Vincent Van Duysen. AVH Offices. Antwerp, Belgium. 2002 (unbuilt). Right: Park Associati. La Serenissima building. Milan, Italy. 2012.

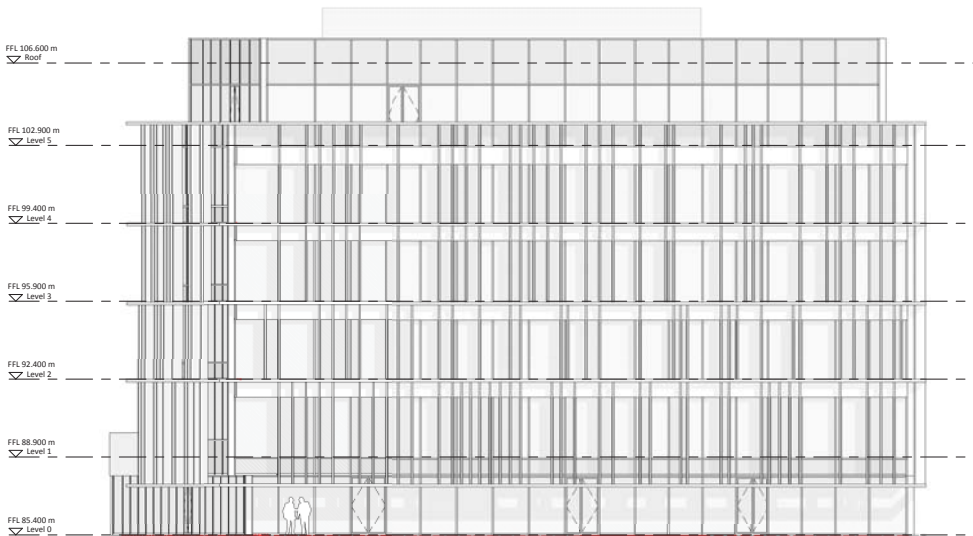


retained adjacent silos. A landscaped deck has been created outside the building's first floor. This deck will form an attractive feature for pedestrians approaching the Wheat Quarter site.

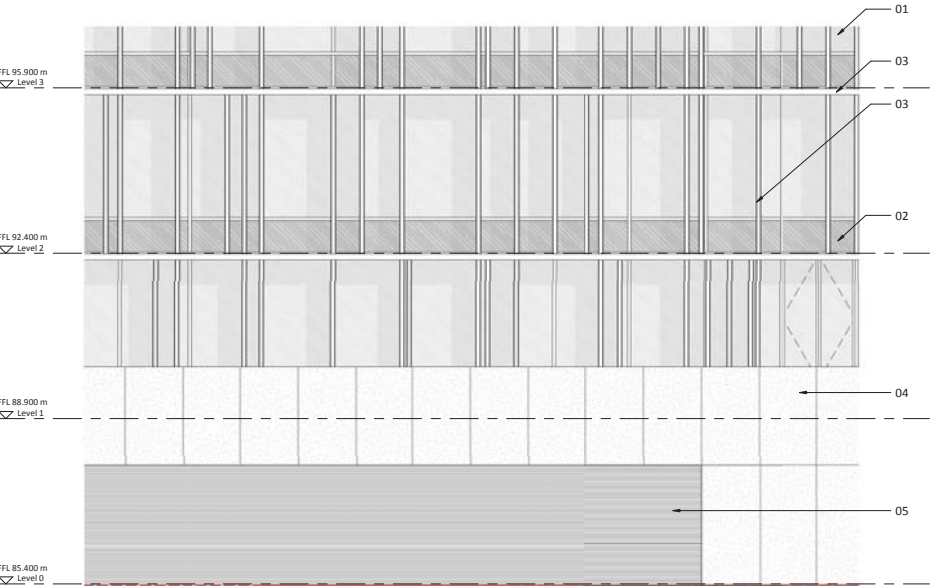


Goodman Square Elevation

Section



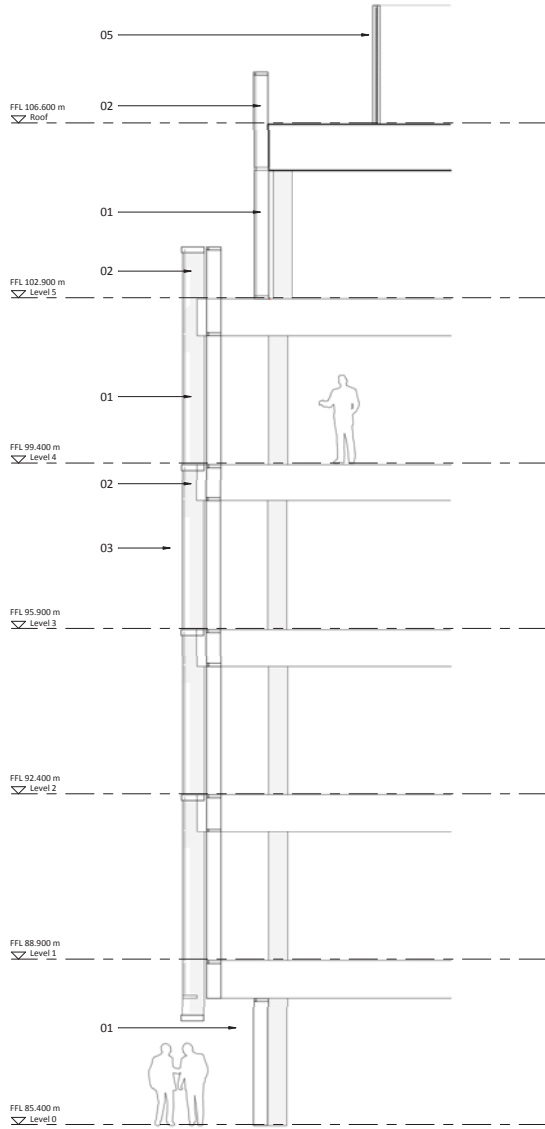
TYPICAL BAY DETAIL



1 Facade Elevation Detail
1 : 50

- Material Notes:
- 01. Double glazed PPC Stick Curtain Wall System
 - 02. Double glazed PPC Stick Curtain Wall System - Back Painted Spandrel
 - 03. GRC (Glass reinforced concrete) vertical and horizontal fin system, smooth finish
 - 04. GRC (Glass reinforced concrete) rainscreen panels, semi-sandblasted finish
 - 05. Screening single bank PPC louvre system

SECTION & MATERIALS



2 Facade Section Detail
1 : 50



THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

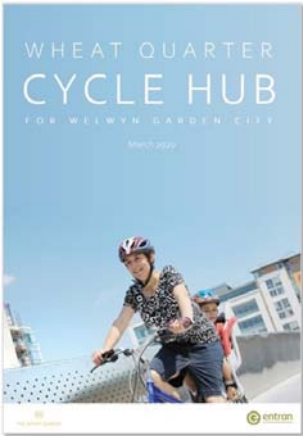
8. DESIGN
THE BRIDGE BUILDING

8.1. THE BRIDGE BUILDING

OVERVIEW

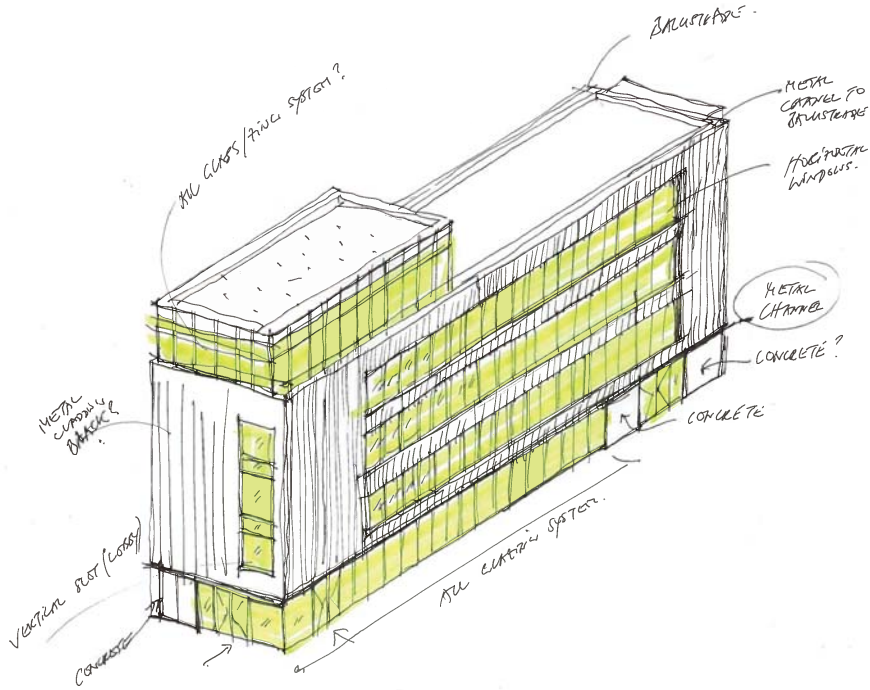
The proposed Bridge building is located adjacent to the raised pedestrian footbridge which gives access to Welwyn Garden City train station and the town centre. The portion of the Wheat Quarter site which this proposal relates to has planning consent for proposes 318m² gross internal space of commercial space.

It is proposed to design a new building that increases the amount of Commercial, Civic and Leisure uses over the previously consented TOC building in the masterplan as well as proposes to host the location of a Cycle Hub on the Ground floor.



The Bridge Building location diagram

CONCEPT SKETCH



8.2. USE & FLOOR PLANS

GROUND & FIRST FLOORS

The ground floor allows for 168m² internal floor space for the proposed Cycle Hub, with a separate circulation zone for the upper floors and a designated area for back of house.

Cycle Hub frontage and access is from the north with a fire exit door to the west elevation. Please refer to 'The Welwyn Garden City Cycle Hub' brochure which will form an appendix to the submission.

Access to the commercial units on

the upper floors is via the entrance lobby to the east of the building.

At first floor level, the unit allows for 224m² of internal floor space for a mixture of community and healthcare uses.

All upper floors have access to the escape stair on the west elevation.

KEY

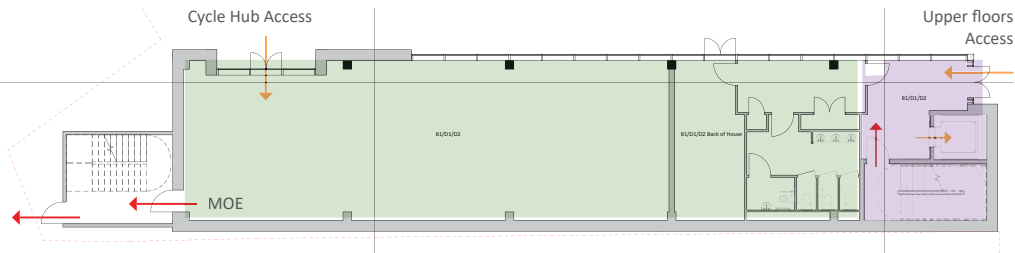
Means of escape

→

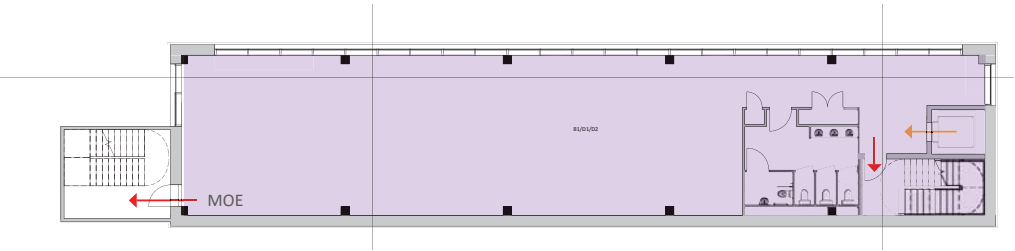
Access

→

Cycle Hub



Ground Floor Plan



First Floor Plan

SECOND TO FOURTH FLOORS

The second and third floors allow for a further 224m² internal floor space each for a mixture of uses.

Access is via the stair and lift core served from the ground floor main entrance. Access to the escape stair is to the rear on the West elevation.

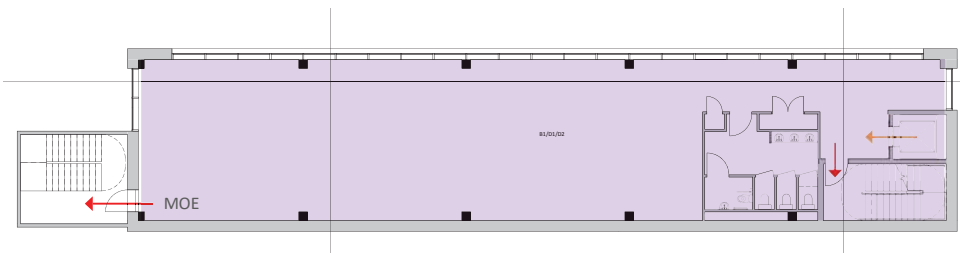
The fourth floor allows for 79m² internal floor space and will most likely form part of the use and demise of the third floor.

The unit benefits from access to a roof terrace.

KEY

→ Means of escape

→ Commercial Access



Second & Third Floor Plan



Fourth Floor Plan

8.3. APPEARANCE

FACADE

The Building envelope is broadly split into three elements.

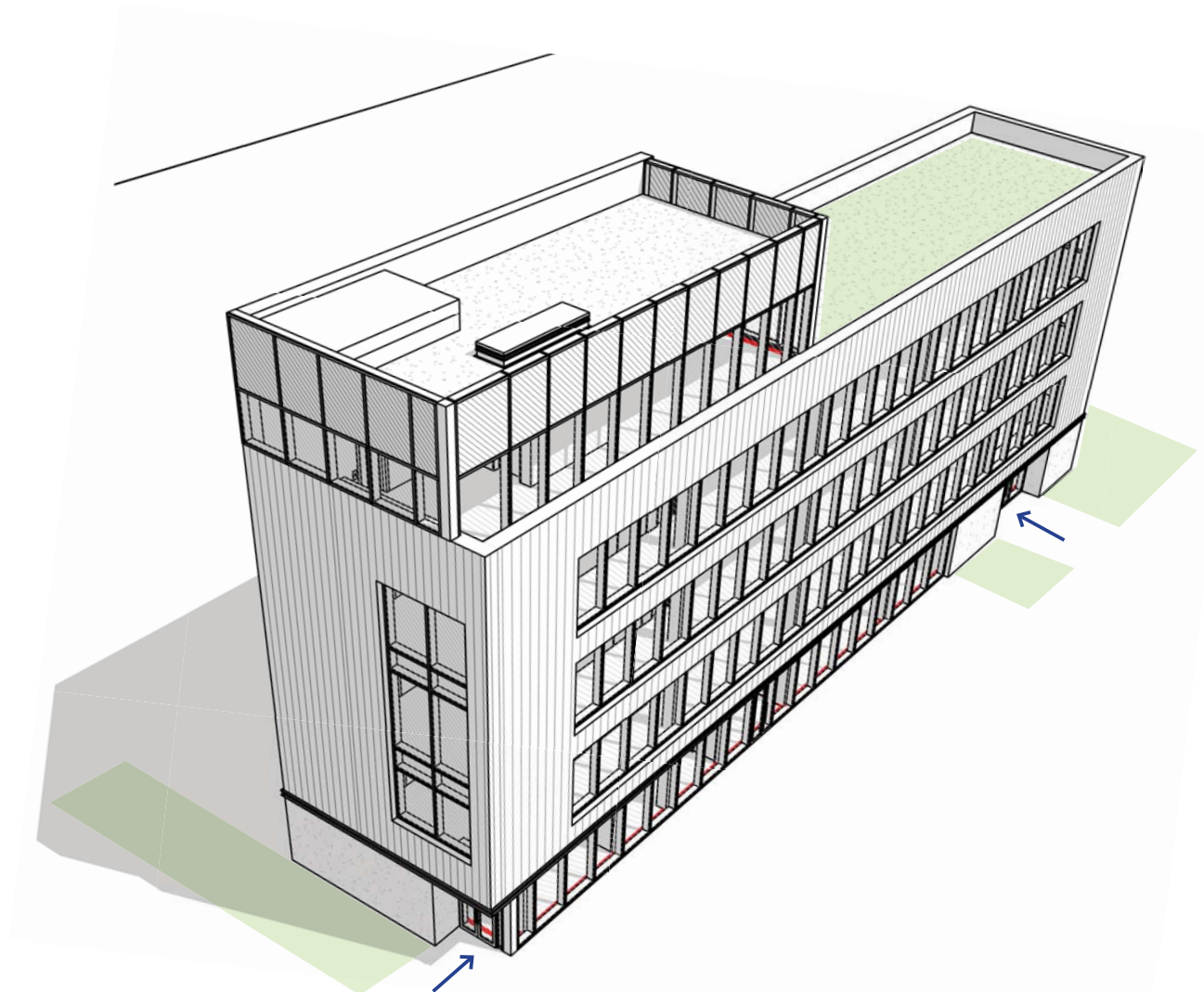
The Ground floor seeks to maximize the active frontage onto Goodman Square and the MTVH site with the main entrance to the upper floors being located on the North-Eastern corner.

The remaining north facing facade offers a maximum of transparency towards the Pedestrian Bridge and Goodman Square.

The typical upper floors are proposed in a metal cladding system which reads as a box and distinctly different from the Ground floor plinth.

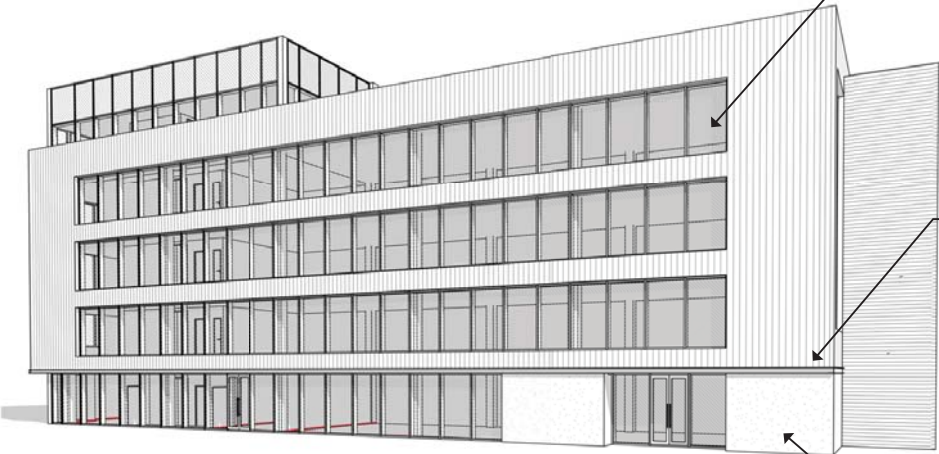
In order to provide access onto a roof terrace and green roof we have proposed a set back lightweight top floor which is to be made of the same glazing system as the Ground floor.

AERIAL VIEW

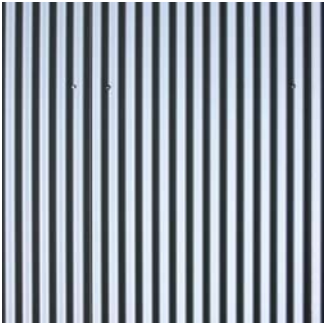


NORTH ELEVATION

MATERIALS



Powdercoated aluminium curtain wall system



Vertical corrugated metal cladding



THE WHEAT QUARTER
DESIGN & ACCESS STATEMENT

9. DESIGN BLOCKS 4&5

9.1. BLOCKS 4 & 5: FORMER SHREDDED WHEAT FACTORY

OVERVIEW

Set at the core of the overall masterplan the primary elements of the former Shredded Wheat Factory buildings are proposed to be reinvented as the new beating heart of the development by introducing a mixture of uses to the Listed buildings by means of comprehensive refurbishment, extension and renovation.

Our proposed designs for the Production hall, as well as Jamie Fobert's for the former Boiler House and Grain silos, seek to build on the current Listed Building Consent and enhance areas in order to facilitate the following uses:

The primary 3 storey factory building known as the Production Hall is

to be converted for a multitude of commercial and leisure uses by introducing a central core to the building which serves all floors and which facilitates flexible division of the building into several uses both laterally as well as vertically.

The Boiler and Grain House are proposed to be converted into a unique Art & Museum Hub with multidisciplinary indoor and outdoor space as well as a bar on the top floor of the silos.

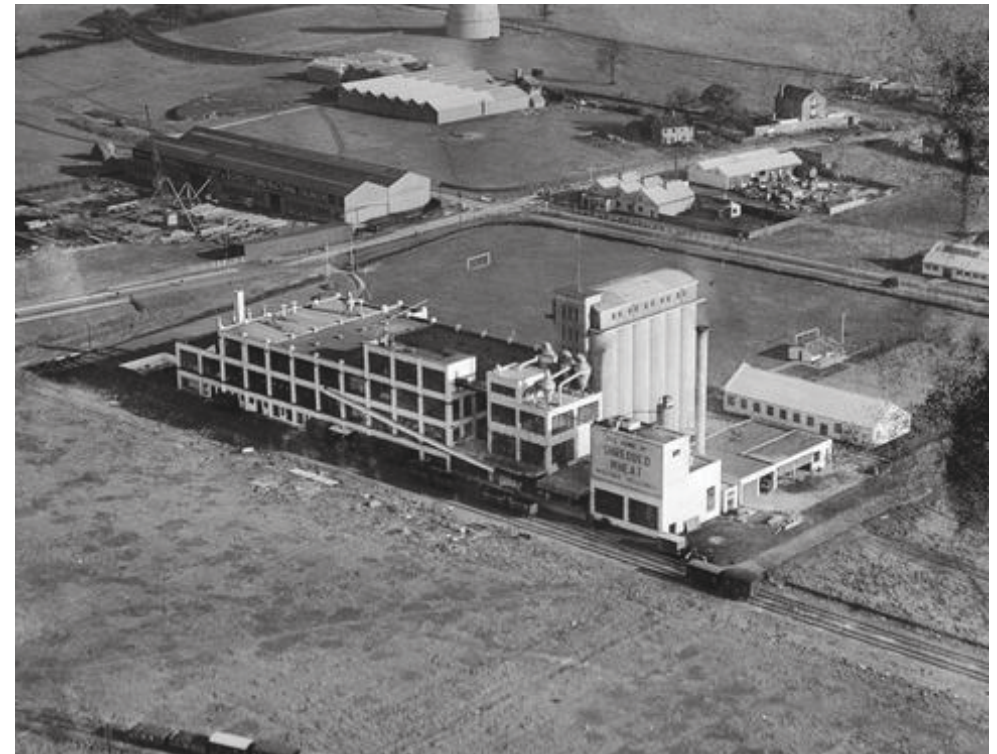
The bar is proposed to be accessed via a new stair and lift core which sits in between the two listed structures and offers the opportunity to link these at upper level.

This chapter should be read in conjunction with the Heritage Statement being prepared by KM Heritage as well as the Design and Access Document provided by Jamie Forbert Architects.

The following pages seek to describe the interventions to the Listed Buildings by assessing their current condition, describing the elements to be removed and proposed departures from the current Listed Building Consent.



Block 4 and 5 location diagram



CURRENT CONDITION

Production at the former Shredded Wheat Factory ceased in 2008 and was consolidated at their plant in Staverton, Wiltshire.

The complex has been unoccupied since Shredded Wheat's move; this was also when most of the internal machinery was removed.

The Wheat Quarter site today consists of the accumulated three main phases of development, with the majority of the original plot now filled with buildings. Much of the original factory is hidden behind the later, less architecturally interesting buildings, including the 1930's Production Hall and the 1950's range of administrative offices along Bridge Road.

Relatively recent refurbishment to the 1920's Factory buildings led to interior and exterior alterations which have had adverse effects on the appearance.

These alterations include:

- The introduction of Seaporcel green panels to the Production Hall covering the spandrel area;
- Replacement of the original Crittall windows with PVC or metal with irregular mullions;
- Standardisation of all upper floor window heights; adding of external stairs to the western façade;
- Removal of windows on the façade which were filled with bricks.

There have also been a number of extensions and additions to the entire complex which, although minor, have compromised the distinct form of the original 1920's factory.

PRODUCTION HALL



THE GRAIN HOUSE, SILOS & BOILER HOUSE

