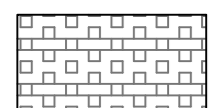

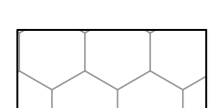

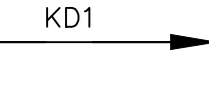
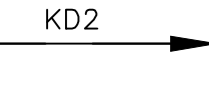
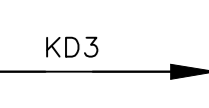



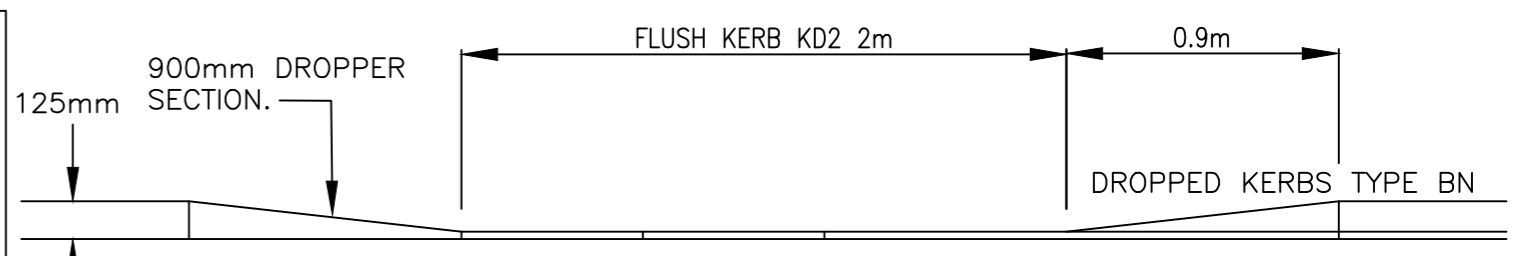
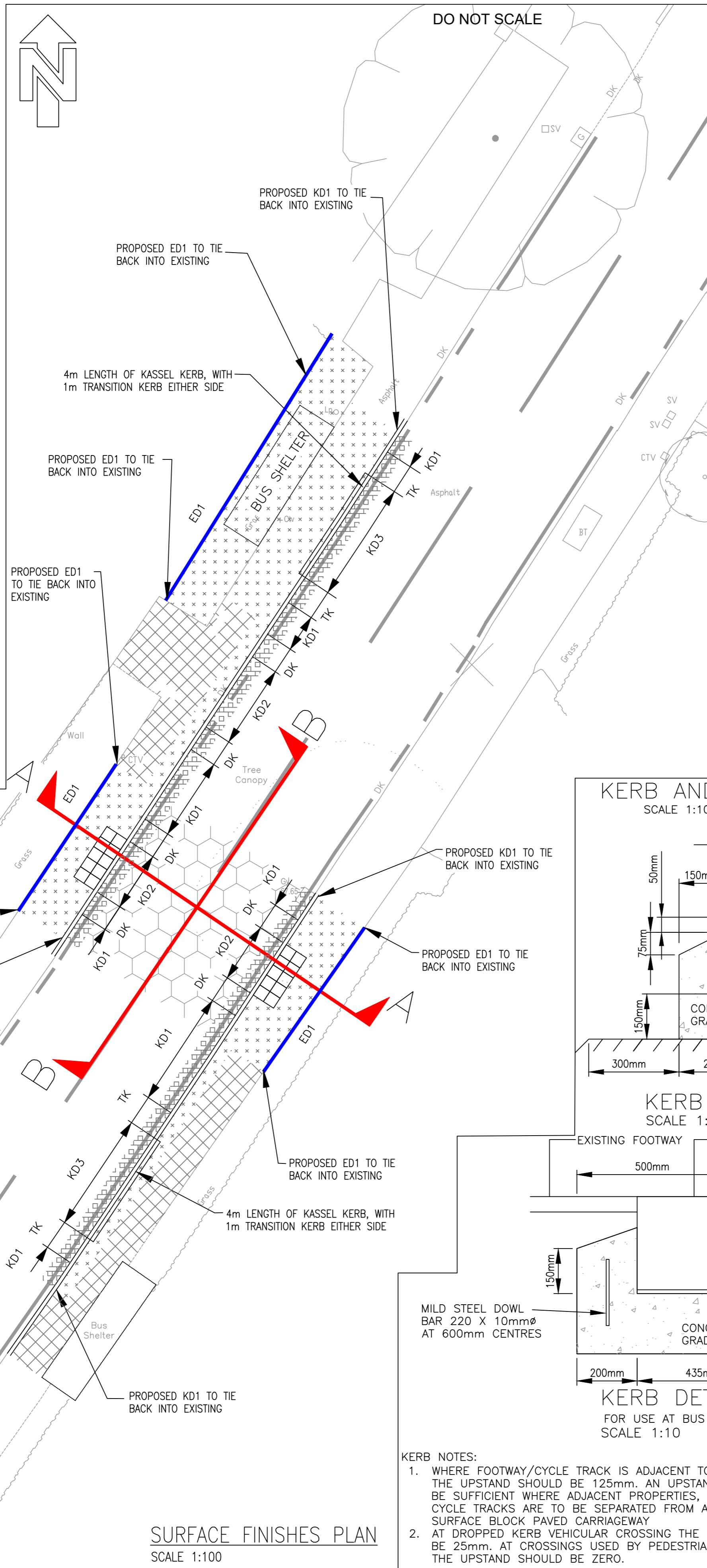
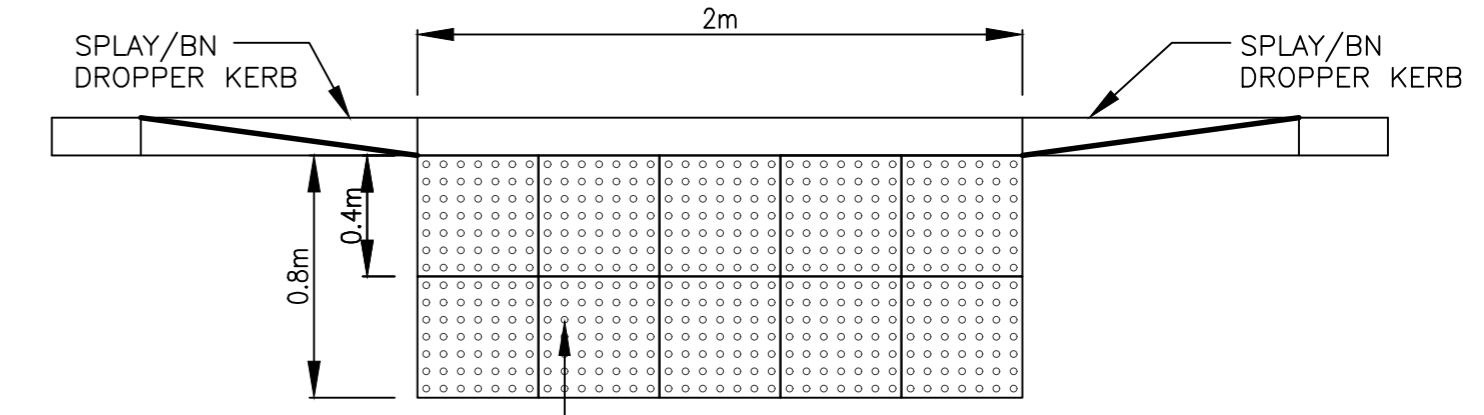


EXTERNAL FINISHES

-  DENOTES EXTENT OF ASPHALT INDUSTRIAL ROAD CONSTRUCTION.
-  DENOTES EXTENT OF NEW ASPHALT FOOTPATH.
-  DENOTES EXTENT OF ROAD RESURFACING WORKS CONSISTING OF NEW SURFACE COURSE
-  DENOTES EXTENT OF FOOTPATH RESURFACING WORKS CONSISTING OF NEW SURFACE COURSE
-  **KD1** NEW 125 x 255 PRECAST CONCRETE HALF BATTERED KERB (TYPE HB2) WITH A 125mm UPSTAND. (KERB DETAIL 1)
-  **KD2** NEW 125 x 150 PRECAST CONCRETE BULLNOSED KERB (TYPE BN) WITH A 0 TO 5mm UPSTAND. (KERB DETAIL 2)
-  **KD3** NEW 435 x 310 KASSELL KERB WITH A 160mm UPSTAND. (KERB DETAIL 3)
-  **ED** NEW 50 x 150 EDGING DETAIL WITH A 50mm UPSTAND (EDGING DETAIL 1)
-  **TK** NEW TRANSITION KERB INSTALLED BETWEEN KERB DETAILS 1 AND KERB DETAILS 3
-  **DK** NEW DROPPER KERB INSTALLED BETWEEN KERB DETAIL 1 AND KERB DETAIL 2.



ELEVATION OF DROPPED PC KERB
SCALE 1:25

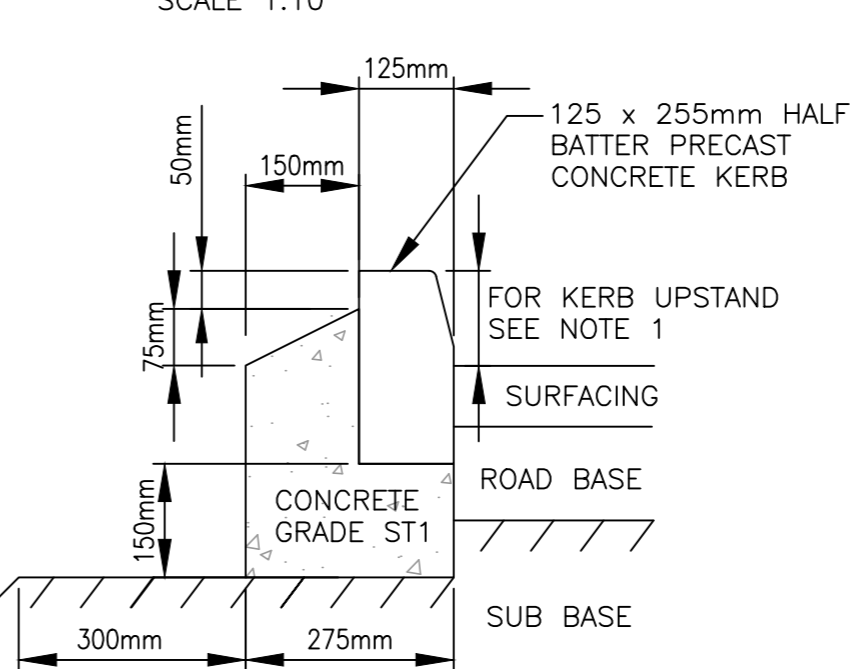


LAYOUT OF TACTILE PAVING AT OFFLINE UNCONTROLLED CROSSING (2m x 0.8m)
SCALE 1:25

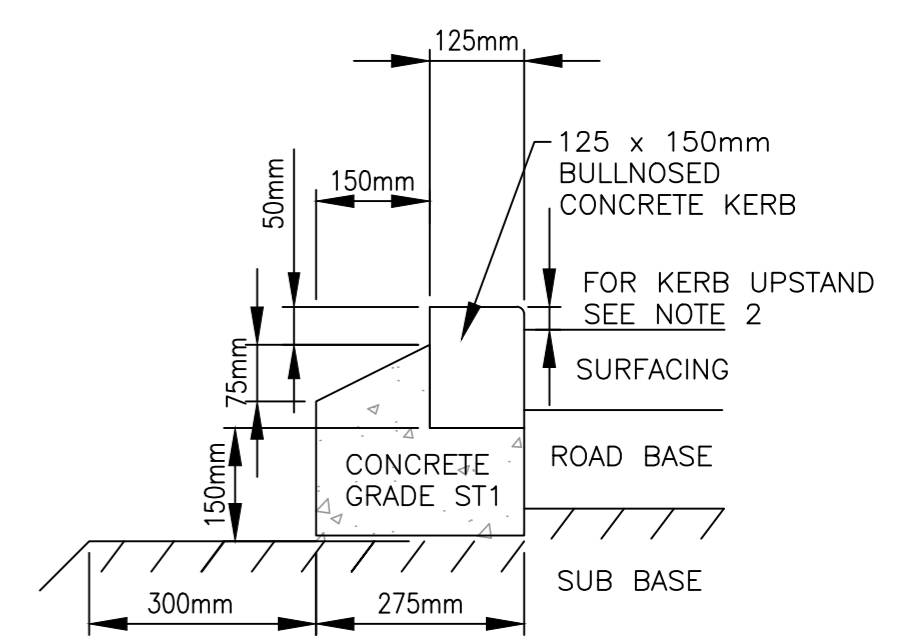
MINIMUM SUB-GRADE AND CAPPING THICKNESS BY CBR VALUE FROM HERTFORDSHIRE HIGHWAYS DESIGN GUIDE 3RD EDITION

CBR VALUE	Minimum Capping Thickness	Minimum Sub-base Thickness
LESS THAN <2.5%	600mm	150mm
2.5-5.0%	-	350mm
2.5-5.0%	350mm	150mm
5.0-15.0%	-	225mm
15.0-30.0%	-	150mm
GREATER THAN >30%	-	-

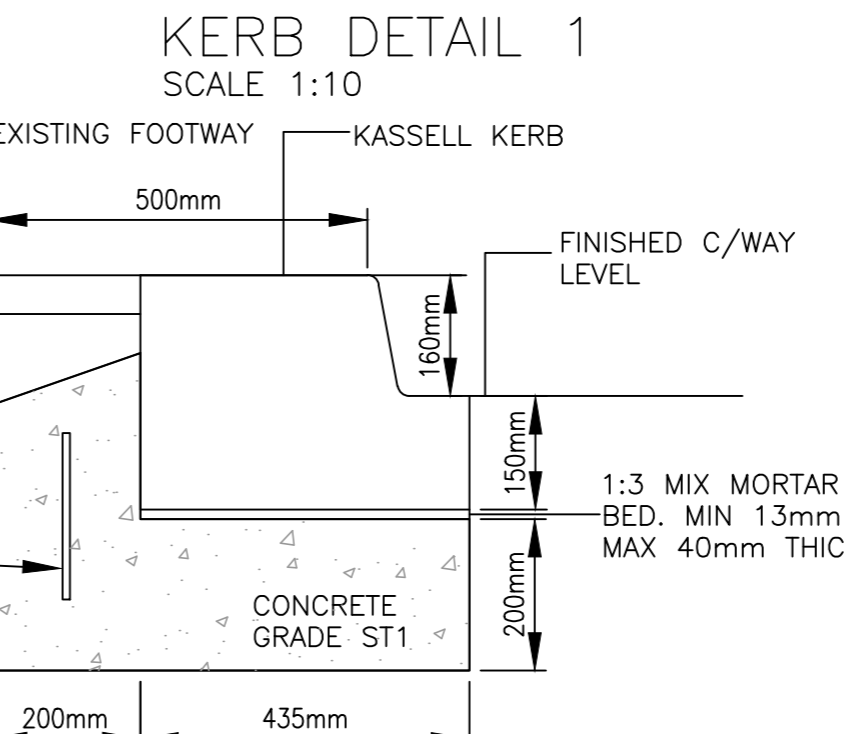
KERB AND EDGING DETAILS
SCALE 1:10



KERB DETAIL 1
SCALE 1:10



KERB DETAIL 2
SCALE 1:10



KERB DETAIL 3
FOR USE AT BUS STOPS
SCALE 1:10

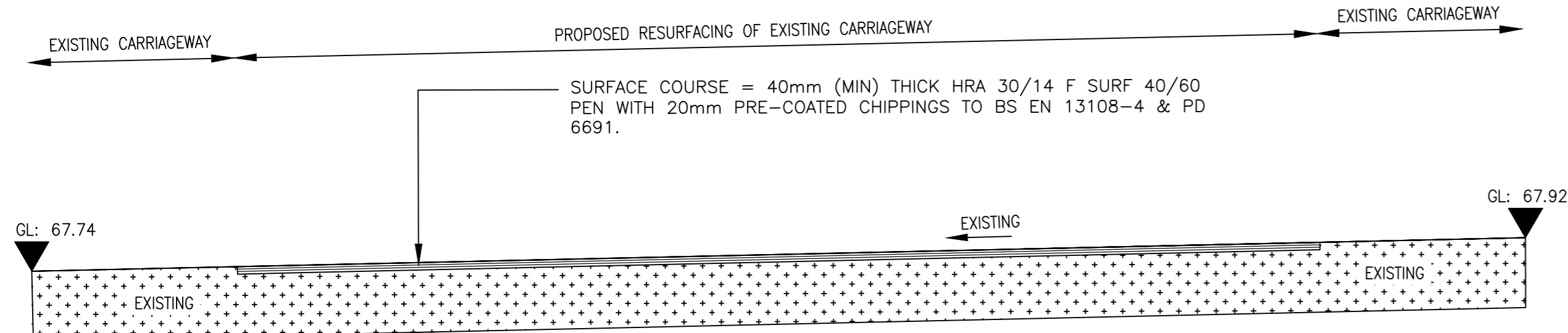
NOTES

- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL RELEVANT DRAWINGS, DOCUMENTS AND SPECIFICATIONS.
- DIMENSIONS NOT TO BE SCALED.
- ALL WORKS SHALL COMPLY & BE CARRIED OUT IN ACCORDANCE WITH HERTFORDSHIRE COUNTY COUNCIL'S DESIGN GUIDES.
- UNLESS NOTED OTHERWISE, CLAUSES REFER TO THE SPECIFICATION FOR HIGHWAY WORKS: VOLUME 1 OF THE MANUAL OF CONTRACT DOCUMENTS FOR HIGHWAY WORKS.
- CARRIAGEWAY CONSTRUCTION AND DEPTH OF CAPPING LAYER IS BASED ON A CBR VALUE OF 2-3%, RECOMMENDED BY GEO ENVIRONMENTAL GROUP.
- REPORT TITLE: PHASE II GEO-ENVIRONMENTAL ASSESSMENT INTERPRETATIVE REPORT
- REPORT NUMBER: KGV-GE2-2015-001
- DATED: JUNE 2015
- CAPPING AND SUB-BASE THICKNESS IN ACCORDANCE WITH HERTFORDSHIRE COUNTY COUNCIL'S DESIGN GUIDES. CBR VALUES TO BE CONFIRMED BY CONTRACTOR PRIOR TO ANY CONSTRUCTION COMMENCING AND REPORTED BACK TO THE ENGINEER.
- BITUMINOUS MATERIAL TO BE IN ACCORDANCE WITH BS EN 13108-1:2006.
- IF SURFACE COURSE IS NOT CONSTRUCTED WITHIN 3 DAYS OF BASE COURSE THEN BASE COURSE MUST BE CLEANED AND BOND COATED PRIOR TO LAYING.

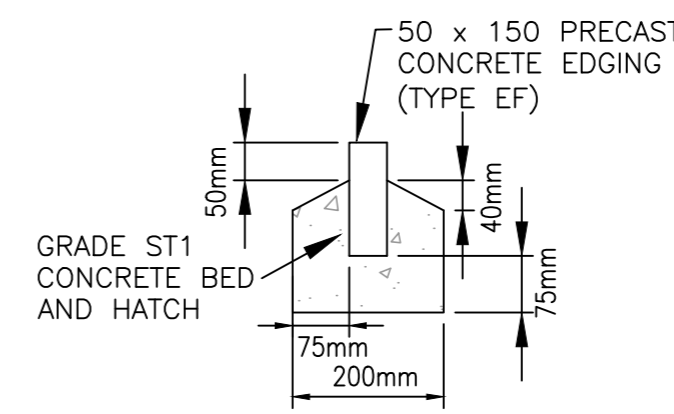
- KERB NOTES:
1. WHERE FOOTWAY/CYCLE TRACK IS ADJACENT TO THE CARRIAGEWAY THE UPSTAND SHOULD BE 125mm. AN UPSTAND OF 40mm WILL BE SUFFICIENT WHERE ADJACENT PROPERTIES, FOOTWAYS AND CYCLE TRACKS ARE TO BE SEPARATED FROM AN AREA OF SHARED SURFACE BLOCK PAVED CARRIAGEWAY
 2. AT DROPPED KERB VEHICULAR CROSSING THE UPSTAND SHOULD BE 25mm. AT CROSSINGS USED BY PEDESTRIANS, OR CYCLISTS THE UPSTAND SHOULD BE ZERO.



SITE LOCATION PLAN
SCALE 1:5000



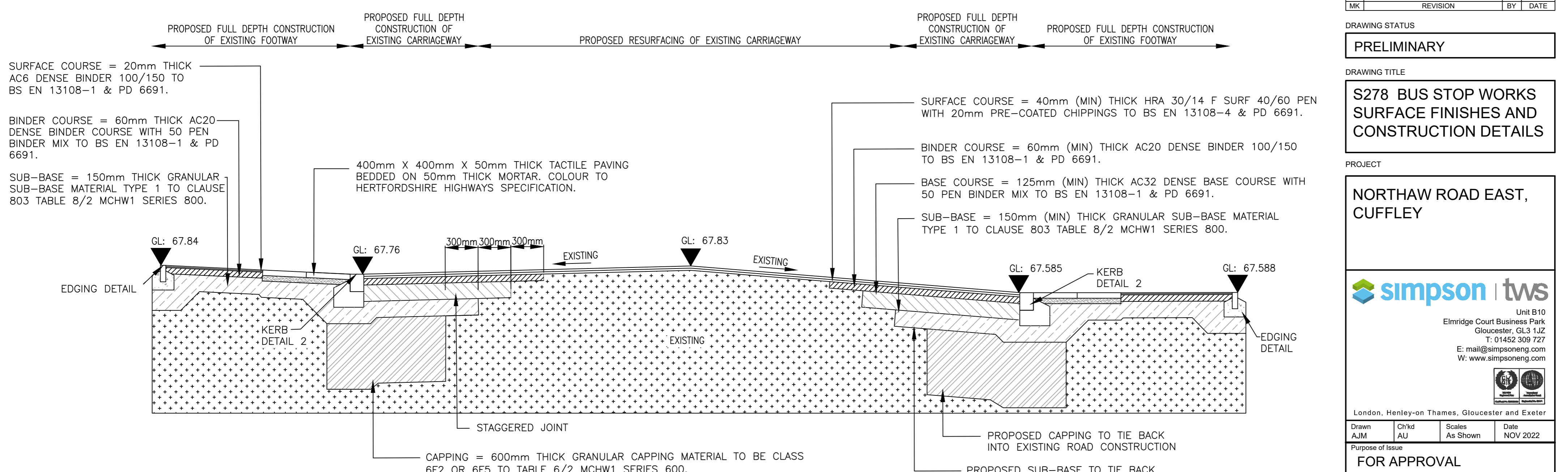
SECTION B - B: SECTION THROUGH CARRIAGEWAY, ILLUSTRATING OVERLAY DETAIL BETWEEN NEW AND EXISTING CARRIAGEWAY
SCALE 1:25



EDGING DETAIL 1
SCALE 1:10

DETAILS AND SPECIFICATION SUBJECT TO S278 APPROVAL FROM HERTFORDSHIRE COUNTY COUNCIL.

PRIOR TO ROAD CONSTRUCTION, HERTFORDSHIRE COUNTY COUNCIL MUST BE CONTACTED TO ESTABLISH REQUIRED CBR TESTING METHODOLOGY. HERTFORDSHIRE COUNCIL TO WITNESS ALL CBR TESTING CARRIED OUT.




SECTION A - A: SECTION THROUGH PROPOSED CROSSING, ILLUSTRATING OVERLAY DETAIL

INITIAL ISSUE	REVISION	AJM	01.11.22
MK		BY	DATE

DRAWING STATUS
PRELIMINARY

DRAWING TITLE
S278 BUS STOP WORKS SURFACE FINISHES AND CONSTRUCTION DETAILS

PROJECT
NORTHAW ROAD EAST, CUFFLEY


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Drawn	Chkd	Scales	Date
AJM	AU	As Shown	NOV 2022

Purpose of Issue
FOR APPROVAL

Project Number	Drawing Number	Revision
P22-772	91	-