



Works in close proximity to the Operational Railway Environment

Construction Method Statement

Date: 05/07/2023

Revision: A

Northaw Road East, Cuffley, Hertfordshire, EN6 4RD



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1 PURPOSE OF THIS DOCUMENT

This document has been prepared to address Reserved Matters Planning Condition 3 (decision notice reference: 6/2022/1774/RM) – works in close proximity to the Operational Railway Environment as described below:

No development shall take place until a construction methodology has been submitted to and approved in writing by the Local Authority. The construction methodology shall demonstrate consultation with the Asset Protection Project Manager at Network Rail. The development shall thereafter be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority. The methodology shall include:

- a) construction methodology;*
- b) earthworks and excavations;*
- c) use of crane, plant and machinery; and*
- d) drainage and boundary treatments*

Reason: To ensure that the development can be undertaken safely and without impact to operational railway safety due to the proximity of the proposed development to the operational railway boundary.

Bellway Homes have been in consultation with Network Rail since September 2022. A site visit was arranged with two staff members from Network Rail, namely Jakeer Mohammad (Construction Manager) and Stanley Bruinvels- Scheme Interface Manager.

During the site visit, proximity to NR asset was discussed as well as the proposed permanent and temporary works. Minutes from the meeting are appended to this document. NR deemed the scheme to be of low risk due to the proposed works being 12m away from their asset. A Basic Asset Protection Agreement (BAPA) was discussed for plots 74-77 which are in close proximity to NR. The BAPA was received 20th of June 2023 from NR and Bellway are in the process of agreeing the terms and signing the agreement.

Please see below timeline of meetings held:

22/09/2022 – Initial Questionnaire completed by Bellway homes

03/10/2022 – General information issued to Network Rail

04/11/2022 – Site visit

21/03/2022 – Draft BAPA received

22/03/2022 – Kick-off meeting

31/05/23 – NR confirmation that asset protection is only concerning plots 74-77

20/06/2023 – Final BAPA received



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2 SITE INTRODUCTION

The site comprises 4.89 hectares of agricultural land to the south of Cuffley. The proposal, submitted in the Reserved Matters application, is for up to 121 dwellings and associated infrastructure.

The site is bounded to the north by existing residential development and the grounds of Cuffley Primary School. A railway line and Northaw Road East (B156) form the eastern and western boundaries respectively. On the opposite side of Northaw Road East, there are three pairs of semi-detached dwellings which are accessed from Colesdale to the north. The southern boundary is defined by a mature hedgerow and tree belt lining the Hertfordshire Way footpath. Beyond the footpath to the south west of the site are the King George V (KGV) Playing Fields and sports pitches, which include three sports pavilions (Cuffley football Club, Tennis Club and Bowls Club), a recreation area with hard surfaced MUGA and an area of formal play equipment.



Image 1. Site Plan

3 PLOTS 74, 75, 76 AND 77

The site is bound by Network Rail on the east side. The train tracks are 12m away from the site boundary. Out of the 121 plots on the development, 4 of these plots (number 74, 75, 76 and 77) are in close proximity to Network Rail Asset. The closest structure is 24.4m away from the tracks as shown in image 2 below.

The design of these plots comprises 2 storey detached houses with either integral or separate garage. The tallest property measures 8.2m from the ground level to the ridge height.

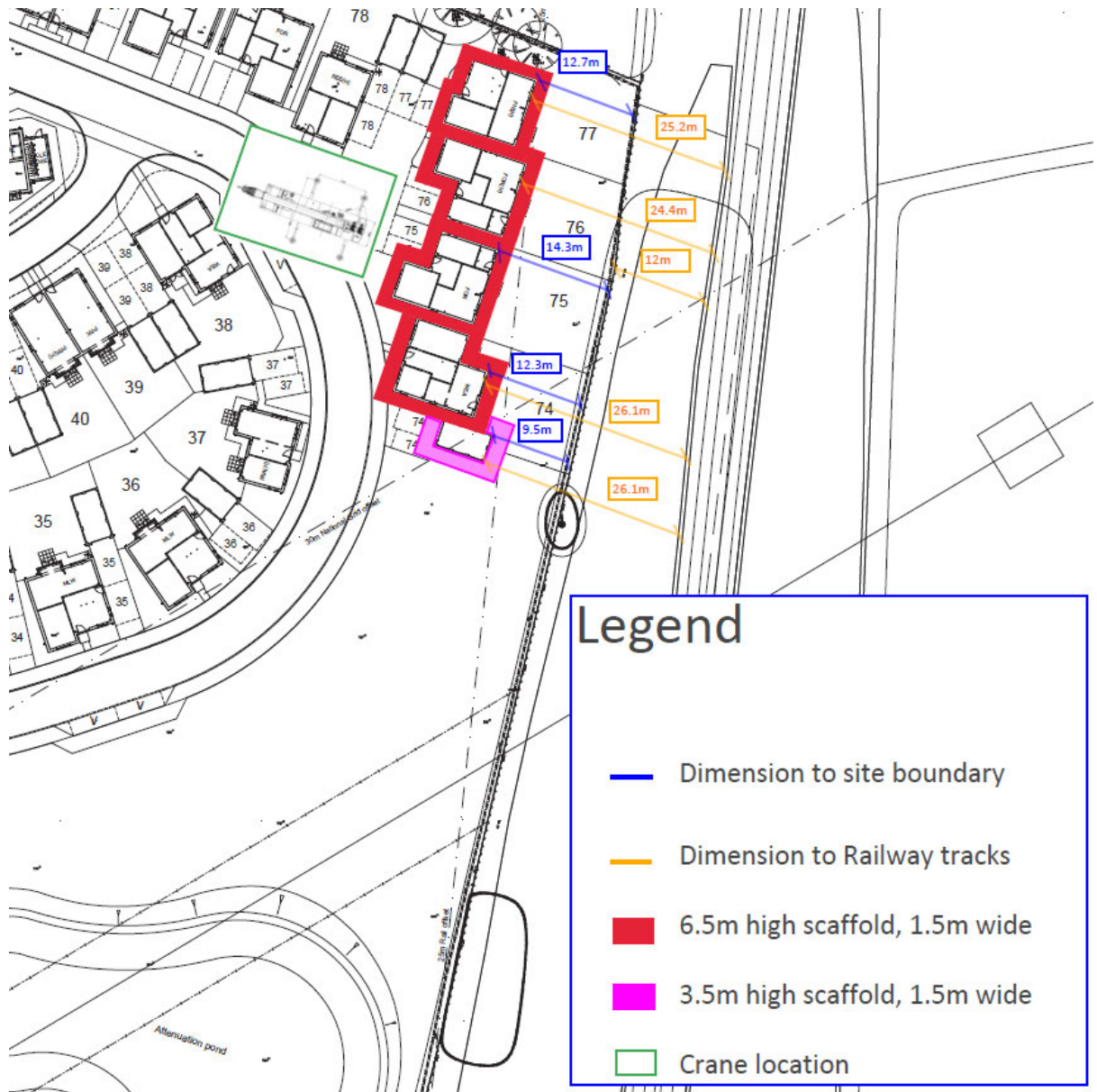


Image 2. Dimension to NR Asset

4 CONSTRUCTION METHODOLOGY

All plots on this scheme, including 74, 74, 76 and 77 are traditionally built (brick and block construction with timber pitched roof). This construction methodology allows for in-situ build. This means that lifting equipment is not required over several days as it would have been in the case of timber frame or SIPS panels for example.

Foundation

Foundation comprises pile foundation to plots 74-77. The closest pile measures 24.4m to the tracks and 9.5m to the site boundary. Pile rig specifications will be confirmed by the pile

External wall construction comprise brick and blockwork with blown insulation. 1.5m wide and 9m high scaffold will be required as shown in image 2. The scaffold will be designed by an appropriately qualified designer who will be made aware of the restrictions via the Pre Construction Information and hazard risk assessment. Scaffold design will be submitted to Network Rail for review and approval.

Roof construction comprise timber trusses and tile. Trusses will be lifted via mobile crane from the west side of the plots, as shown in image 2.

No excavations are proposed to plots 74-77. There is an element of filling between 0.49m and 1.19m in this area as shown in image 4 below.

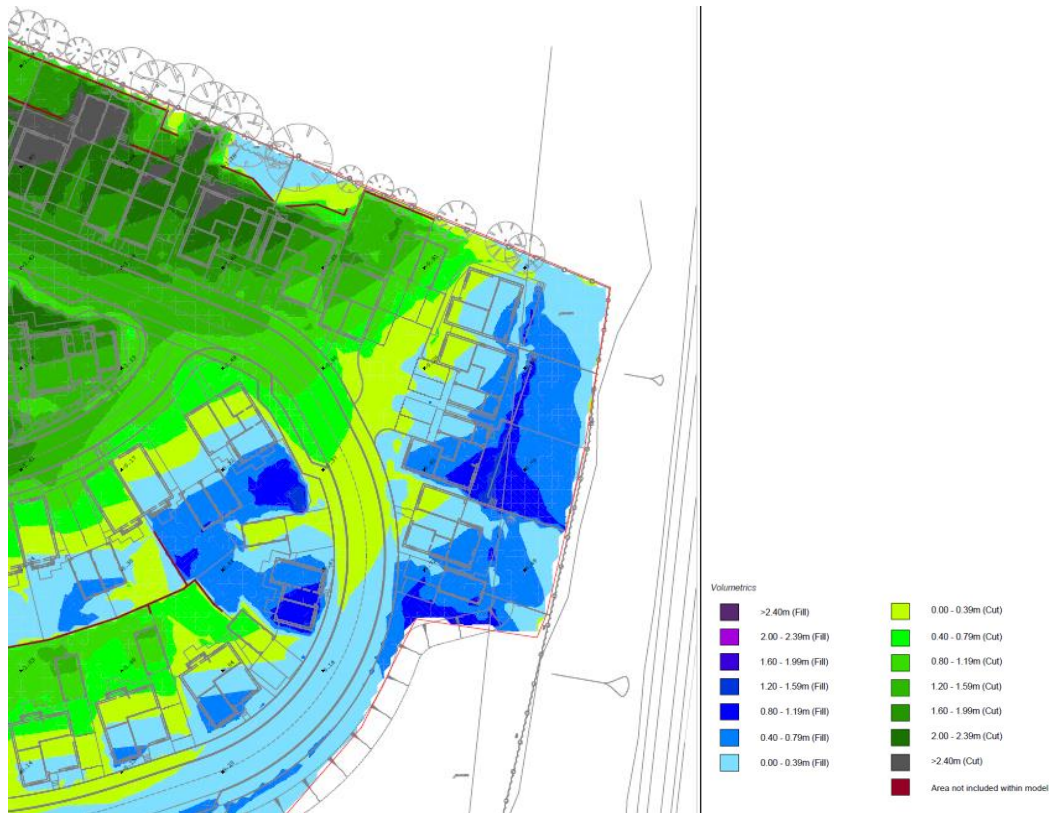


Image 4 – cut and fill

6 USE OF CRANE, PLANT AND MACHINERY

Mobile crane will be used to lift roof trusses in place. This will be located on the west side of the plots and will be 43m away from the train tracks. The reach of the crane is 30m and this is 13m away from the train tracks and 3m away from the site boundary.

Bulldozers and graders will be required for the fill works in the area. These will be located west of the plots. Additional grading work will be done manually to ensure plant equipment does not come closer than 15m to the train tracks. The contractor will be made aware of restrictions regarding plant and machinery via Pre Construction Information and Hazard Risk Assessment. RAMS will be submitted to Network Rail for review and approval.

Pile rig will be required as described above.

Submission of temporary works design will be completed by an adequately qualified designer and submitted to Network Rail for approval.

The surface water drainage discharges into manholes located on the road, west of the plots. This connects to the wider network which discharges into the attenuation pond located south – east on site. There are land drains included in back gardens to ensure water discharges safely and gardens are not flooded.

It is worth noting that gardens to plots 74-77 are approximately 2m below the train tracks. Therefore, in a flood event or during construction water will not discharge onto the tracks.

The foul water drainage also discharges into manholes located in the road, west of the plots. This connects into the wider network which connect to the existing Thames Water sewer on site.

Image 5 below shows the drainage network.



Image 5 – drainage

Boundary treatments to plots 74-77 comprise a 2m high density acoustic timber fence. This fence will be maintained by occupiers of Bellway development. The existing metal fence owned by Network Rail will remain in place. The proposed Bellway fence will be located 300mm away from the existing fence as requested by Network Rail. Boundary treatments are shown on image 6 below.

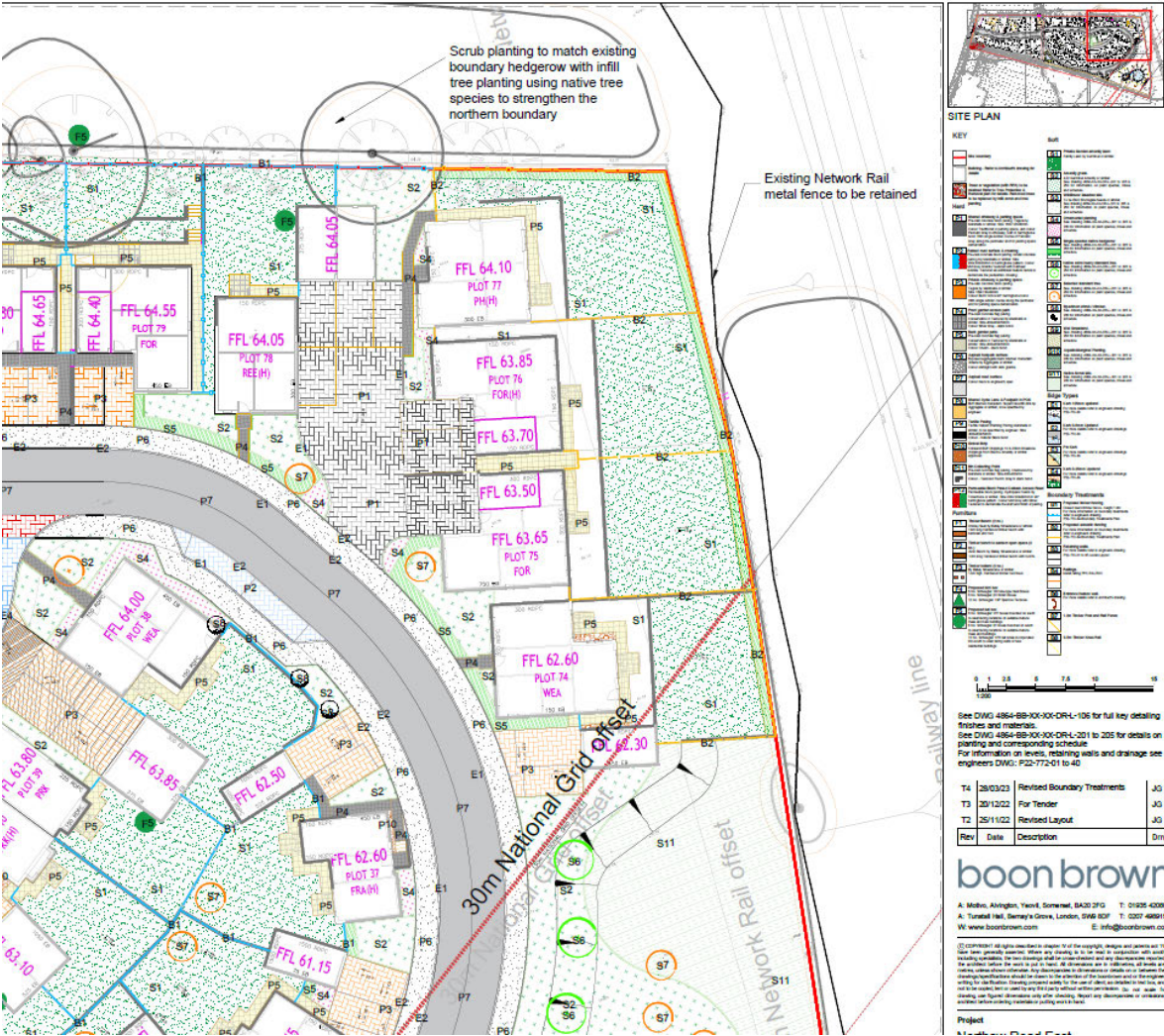


Image 6 – boundary treatments



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Mara Dumitru

From: Network Rail ASPRO: no-reply <erjy.fa.sender@workflow.mail.em2.cloud.oracle.com>
Sent: 28 September 2022 08:51
To: Mara Dumitru
Subject: Auto- Response from Network Rail to confirm Enquiry - 0000242187, BE01 - King George's Vale

ALERT: This message originated outside of Bellway's network. **BE CAUTIOUS** before clicking any link or attachment.

Dear Mara Dumitru,

Thank you for submitting your Enquiry/ Application, this is an automated response to confirm it has been received into our system to the Anglia Asset Protection Team.

Our reference for your Enquiry/ Application is - 0000242187

We aim to respond with the name of your assigned Network Rail Interface Manager within 5 working days.

If you haven't had a response from us, or should you wish to discuss your enquiry with a member of the team, please contact the Asset Protection Team.

By phone: 07922 017393

By email: AssetProtectionAnglia@networkrail.co.uk

By post: Asset Protection Team, Network Rail, 11th Floor, One Stratford Place, Montfichet Road, Stratford, London, E20 1EJ

For more information about the Asset Protection Teams please visit: <https://www.networkrail.co.uk/running-the-railway/looking-after-the-railway/asset-protection-and-optimisation/>

Kind Regards

Anglia Asset Protection Team

Note : This is an auto generated email. Please do not reply to this email.

Mara Dumitru

From: Iqra Zaman [REDACTED]
Sent: 20 March 2023 15:36
To: Mara Dumitru
Cc: Stanley Bruinvels
Subject: 0000242187/SB Basic Asset Protection Agreement: Cuffley, Northaw Road East
Attachments: 0000242187 Basic Asset Protection Agreement.pdf

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OFFICIAL

Good Afternoon,

Please find attached estimate of costs and Basic Asset Protection Agreement (BAPA) in respect of the above works. We would be pleased if you could arrange the signing and dating of Page 10 of the BAPA and return the full document, together with your confirmation of Purchase Order confirming acceptance of costs, and your Public Liability Insurance to asprolne&emcommercial@networkrail.co.uk (email only please due to current situation). Once we have received all documents a copy of the BAPA, signed on behalf of Network Rail, will be arranged to be returned to you.

You should be aware that this letter is not an offer to contract. We will advise you when we have our authorities in place and can enter into a contract. No site works should commence before you have been advised of this. We will advise you of this by countersigning the BAPA and returning a copy of the duly signed agreement to yourselves.

Iqra Zaman
Assistant Commercial Support
Contracts & Procurement – Asset Protection (Eastern)
[REDACTED]

W: www.networkrail.co.uk



Coronavirus Working Arrangements
Our Asset Protection Team have taken action in line with Government advice to ensure we adhere to the social distancing measures. Asset Protection will endeavour to continue as usual but there may be occasions where we are unable to meet our usual timescales, but we will make every effort to keep you informed.

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Mara Dumitru

From: Stanley Bruinvels [REDACTED]
Sent: 29 March 2023 13:47
To: Mara Dumitru; Leirah Nakamaanya (Asset Protection Engineer)
Cc: Steve Cutting; Owen Mesley
Subject: RE: Cuffley - Dimensions to asset 242187

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OFFICIAL

Hi Mara

Thanks for the attached.

Since the meeting I've had a quick discussion with the senior project team on how they think we should take this forwards.

The advice seems to be that at face value it looks as if the project does have little involvement in the railway, but it would be worth getting a site visit in the diary when the scaffolding/crane/hoarding is set up just to verify distances on site.

So I think here it would be worth getting the contract signed off, and then we'll issue the LONO to take us forwards.

But all going well there would be very little involvement from NR, just a site visit to verify all is how it should be.

How does that sound?

Thanks

Stanley



From: Mara Dumitru [REDACTED]
Sent: 26 March 2023 18:27
To: Leirah Nakamaanya (Asset Protection Engineer) <Leirah.Nakamaanya2@networkrail.co.uk>; Stanley Bruinvels [REDACTED]
Cc: Steve Cutting [REDACTED]; Owen Mesley [REDACTED]
Subject: Cuffley - Dimensions to asset

Hi Stanley and Leirah,

It was great to meet you last week to discuss Cuffley.

As requested, please find attached plan to show dimensions to the site boundary and railway tracks.

This also shows the location and height of scaffold as well as an indicative location of the crane.

You will note that the worst case scenario notes a distance of 25.2m to the tracks. In the event that the scaffold would fall, this will be within the Bellway site and would not even reach the site boundary between Network Rail and Bellway.

Further to our conversation last week and following receipt of the attached drawing, please could you confirm if a letter of no objection can be sent?

We appreciate your input on this project and time spent reviewing the information and we look forward to reaching a suitable agreement for both parties on this scheme.

Kindest Regards,

Mara Dumitru
Senior Technical Coordinator

01895 671196
Bellway Homes Limited (North London)
Bellway House
Bury Street
Ruislip



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Mara Dumitru

From: Stanley Bruinvels [redacted] >
Sent: 31 May 2023 11:36
To: Town Planning LNE; T&CP Aspro LNEEM
Cc: Fadzai Dera; Ashley Ransome; Owen Mesley; Mara Dumitru
Subject: RE: 6/2022/1774/RM- Land Adj King George V Playing Fields, Northaw Road East, Cuffley, Hertfordshire, EN6 4RD

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Hi Matt,

Just on the topic of planning conditions, from a NR asset protection point of view we would be happy with the wording below:

No development of the plots next to the railway (74-77) shall take place until a construction methodology has been submitted to and approved in writing by the Local Authority. The construction methodology shall demonstrate consultation with the Asset Protection Project Manager at Network Rail. The development shall thereafter be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority. The methodology shall include:

- a) construction methodology;*
- b) earthworks and excavations;*
- c) use of crane, plant and machinery; and*
- d) drainage and boundary treatments*

Previously we've been to site with the team and assessed the risks, we have no major concerns with other parts of the site.

Hopefully this will allow the team to start at the opposite end of the site which is well away from the railway.

Thanks
Stanley



From: Ashley Ransome [redacted]
Sent: 25 May 2023 14:42
To: Town Planning LNE <TownPlanningLNE@networkrail.co.uk>
Subject: 6/2022/1774/RM- Land Adj King George V Playing Fields, Northaw Road East, Cuffley, Hertfordshire, EN6 4RD

OFFICIAL

Dear Matt,

I write in reference to the application which was granted permission at the above site under reference 6/2022/1774/RM for which you provided the attached comments.

I have been in contact with Stanley with regards to a non-material minor amendment application that has been submitted to change the wording of one of the conditions that Network Rail requested. Please see the below two emails where I set out the application and my concerns, and Stanley's response. I was wondering if you would be able to help move this along please?

Kind Regards

Ashley Ransome
Senior Development Management Officer
Welwyn Hatfield Borough Council

01707 357392
[redacted]

welhat.gov.uk



From: Stanley Bruinvels [REDACTED]
Sent: 23 May 2023 09:51
To: Asprolne&emcommercial <Asprolne&emcommercial@networkrail.co.uk>
Cc: Jason Thomas-Rhoden [REDACTED]; Fadzai Dera [REDACTED]; Ashley Ransome [REDACTED]
Subject: FW: Land Adj King George V Playing Fields, Northaw Road East, Cuffley, Hertfordshire, EN6 4RD

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OFFICIAL

Thanks for your message Ashley, our commercial team [@Asprolne&emcommercial](mailto:Asprolne&emcommercial@networkrail.co.uk) will be much better placed to answer your queries rather than myself.

Commercial team – what do you think of the below?

Thanks
Stanley



Stanley Bruinvels
(Pronouns: He/Him/His)
Asset Protection South
[REDACTED]

From: Ashley Ransome [REDACTED]
Sent: 22 May 2023 15:31
To: Stanley Bruinvels [REDACTED]
Subject: Land Adj King George V Playing Fields, Northaw Road East, Cuffley, Hertfordshire, EN6 4RD

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Dear Stanley,

I have been passed your details by Bellway with respect to the above site, for which I am dealing with a non-material minor amendment (NMA) application. The application seeks to amend the wording of a condition Network Rail requested on the reserved matters application of 6/2022/1774/RM.

The current wording of the condition reads:

No development shall take place until a construction methodology has been submitted to and approved in writing by the Local Authority. The construction methodology shall demonstrate consultation with the Asset Protection Project Manager at Network Rail. The development shall thereafter be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority. The methodology shall include:

- a) construction methodology;*
- b) earthworks and excavations;*
- c) use of crane, plant and machinery; and*
- d) drainage and boundary treatments*

Bellway have requested for the condition to be reworded to the below:

No development of the plots next to the railway (74-77) shall take place until a construction methodology has been submitted to and approved in writing by the Local Authority. The construction methodology shall demonstrate consultation with the Asset Protection Project Manager at Network Rail. The development shall thereafter be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority. The methodology shall include:

- a) construction methodology;*
- b) earthworks and excavations;*
- c) use of crane, plant and machinery; and*
- d) drainage and boundary treatments*

I have raised two concerns with regards to this:

1. The process of dealing with this change. Under an NMA application, no consultees are consulted and thus in this case, yourselves, Network Rail, are not consulted so do not have the opportunity to provide comments on a condition that you specifically requested during the reserved matters application.
2. There is concern because whilst the plots next to the railway may not be the first to be built out, the potential risk to the railway line still exists due to other potential onsite activity.

Bellway came back to me on the above concerns advising that they have been in contact with yourself. I was wondering if we could discuss the change to the wording of this condition, and if you can provide me with any guidance please.

Kind Regards

Ashley Ransome
Senior Development Management Officer
Welwyn Hatfield Borough Council

01707 357392
[Redacted]

welhat.gov.uk



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Mara Dumitru

From: Iqra Zaman [REDACTED]
Sent: 12 June 2023 11:23
To: Mara Dumitru
Cc: Stanley Bruinvels
Subject: 0000242187/SB Basic Asset Protection Agreement: Cuffley, Northaw Road East
Attachments: 0000242187 Basic Asset Protection Agreement.pdf

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Good morning,

Please find attached estimate of costs and Basic Asset Protection Agreement (BAPA) in respect of the above works. We would be pleased if you could arrange the signing and dating of Page 10 of the BAPA and return the full document, together with your confirmation of Purchase Order confirming acceptance of costs, and your Public Liability Insurance to asprolne&emcommercial@networkrail.co.uk (email only please due to current situation). Once we have received all documents a copy of the BAPA, signed on behalf of Network Rail, will be arranged to be returned to you.

You should be aware that this letter is not an offer to contract. We will advise you when we have our authorities in place and can enter into a contract. No site works should commence before you have been advised of this. We will advise you of this by countersigning the BAPA and returning a copy of the duly signed agreement to yourselves.

Iqra Zaman
Assistant Commercial Support
Contracts & Procurement – Asset Protection (Eastern)
[REDACTED]

W: www.networkrail.co.uk



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Mara Dumitru
Bellway Homes
Building 2030
Cambourne Business Park
Cambourne
CB23 6DW

Network Rail
Asset Protection
George Stephenson House
Floor 3A
Toft Green
York
YO1 6JT

[REDACTED]
ACE Ref: 242187

Subject to contract

Date: 11/11/2022

Dear Mara

Re: 242187, Cuffley, Northaw Road East

With reference to the above project, please find the attached guidance notes and meeting minutes as discussed at the initial site visit held on 04/11/2022. The meeting was held to discuss the provision of safety management to enable your works to be carried out in proximity of the operational railway.

It will be necessary for Network Rail to be involved in the planning, design and site work phases of the proposed works.

From the information gained at the meeting an estimate of costs has been passed to Network Rail's Finance Department and will be sent to you.

In order to progress the scheme, the following information is required:

1. Your written acceptance that you will abide by the conditions given by Network Rail by returning the Basic Asset Protection Agreement BAPA.
2. A Purchase Order confirming your acceptance of the estimated costs.
3. Evidence that you carry the appropriate level of Public Liability insurance.

You should be aware that this letter is not an offer to contract. We will advise you when we have our authorities in place and can enter into a contract. No site works that could impact on the railway should commence before you have been advised of this. We will advise you of this by countersigning the BAPA and returning a copy of the duly signed agreement to yourselves.

To prevent unnecessary expenditure being incurred it is essential that you notify this department if your works are not to proceed. Additionally, any delay to the start of this project or Network Rail receiving acceptance of costs could affect the estimate and result in a possible rate increase.

Yours sincerely

Stanley Bruinvels
Scheme Interface Manager
Asset Protection, Network Rail

Meeting Minutes

Project title: 242187 Cuffley, Northaw Road East

Location of works: Northaw Road East, Cuffley, EN6 4RD ELR and Mileage: HDB @12M 1218Yds

Date of Meeting: 04/11/2022

Attendees:

Mara Dumitru – Senior Technical Coordinator, Bellway Homes

Jakeer Mohammad- Construction Manager, Asset Protection, Network Rail

Stanley Bruinvels- Scheme Interface Manager, Asset Protection, Network Rail

Item No.	Minute	Action Owner
1.	<p>Description of the Scheme:</p> <ul style="list-style-type: none"> Bellway Homes are developing a large section of land at Cuffley. Whilst there are a considerable number of homes planned for the site 3 are in close proximity to the railway. The 3 houses will be in close proximity to railway line approximately ½ a mile South of Cuffley station. All three buildings are expected to be 3 stories In the initial application the houses are thought to be 15m away from the railway line on the Eastern boundary of the site. The development includes construction works such piling works (foundations), , scaffolding, façade, and roofing works. The works will include the use of a mobile crane, which will need to take into account the overhead power cables (not owned/maintained by NR) It is important to note there is no intention to over sail the railway as part of designs Additional proposals that will require Network Rail reviewal include any amendment to retaining walls as part of the project. Between the site boundary and the NR fencing, there is expected to be hoarding to provide an additional boundary. NR have advised that a 300mm gap is left between the two fences so that each side can maintain their boundary fence. In terms of planning consents, the landowner is going through the process to receive planning permission. As the project is in pre planning stage, a feasibility estimate will be produced based on the initial stages up to Approval in Principle. Network Rail have requested a copy of outlined proposal and programme. for Network Rail project teams' reference. The Asset Protection team will compile an estimate based on pre planning stage, this will be issued by our Commercial team in the form of a Basic Asset Protection Agreement. 	
2.	<p>Guidance Notes:</p> <p>You were previously provided with a set of Network Rail Guidance Notes which were deemed relevant to your scheme based on the information you provided with your original enquiry. These were discussed during the meeting and any clarifications are noted below.</p> <p>Additional Guidance Notes were discussed at this meeting as listed below:</p> <ul style="list-style-type: none"> ASPRO.GN001 General Guidance Note works on or adjacent to the railway February 2022 ASPRO.GN002 Access, minor works and inspections ASPRO.SG005 Developments & maintenance works adjacent to the operational railway Feb 2022 ASPRO.SG006 Demolition Feb 2020 	

	<ul style="list-style-type: none"> • ASPRO.SG007 Scaffolding • CPA-Mobile-Cranes-Alongside-Railways-1801 	
3.	<p>Confirmation of Asset Protection services:</p> <p>On receipt of the signed BAPA and a purchase order number. Network Rail will progress:</p> <ul style="list-style-type: none"> a. Provision of supervisory staff including railway safety critical staff b. Review of permanent designs c. Review of temporary designs d. Checking method statements. e. Issue access permits f. Attendance at meetings g. Liaise with other Network Rail departments 	
4.	<p>General Construction Information:</p> <p>It was deemed that Network Rail would require information and to oversee several of the operations on site, these are as follows;</p> <ul style="list-style-type: none"> • Scaffold erection • Crane operations • Works in relation to the NR retaining wall • Temporary works • Any other operations that may affect the safety of the operational railway. 	
5.	<p>Proposed temporary/permanent works design submissions:</p> <p>The following are attached. FormC, F0039 & F0040 accompanied by briefing notes for your future reference.</p> <ul style="list-style-type: none"> • Temporary Works for Scaffolding, retaining wall and mobile crane working platform • Form F0039 Appointment of Contractors Engineering Manager (CEM). • Form F0040 Appointment of Contractors Responsible Engineer (CRE). <p>There is a 28 day lead time for Engineering reviews.</p>	
6.	<p>Proposed RAMS submissions:</p> <p>At feasibility stage:</p> <ul style="list-style-type: none"> • Site investigation works • Construction works in close proximity to NWR infrastructure <p>This list is not exhaustive; but shall include all works that have the potential to have an impact on Network rail infrastructure. We need 21 days to review MS and to arrange site supervision on site, for which we require 10 days. A Method statement ACCEPTED BY Network Rail must be in place 10 days prior to work commencing on site.</p> <p>A Method Statement cover sheet is attached.</p> <p>There is a 20 day lead time for RAMS reviews, with a 10-day cooling off period.</p>	
7.	<p>Site specific Possession and Isolation Information:</p> <p>Not required.</p>	
8.	<p>Site specific Line Blockages Information:</p> <p>Not required</p>	
9.	<p>Site accommodation may be required for the Network Rail representative which will include:</p>	

	<ul style="list-style-type: none"> • A desk • Lockable filing cabinet • Phone/broadband connection • Welfare facilities • Access to dry room • Parking provision • Please confirm when access to the above will be possible. 	
10.	<p>Network Rail's associated costs:</p> <p>The Client will be required to enter into a Basic Asset Protection Agreement (BAPA), under the terms of which any costs incurred by Network Rail in connection with the works will be reimbursed. An Agreement and associated estimate of Network Rail's costs will be prepared as detailed below:</p> <p>It was confirmed at the meeting that NR will estimate for the following activities:</p> <ul style="list-style-type: none"> • The review of a single design and provide comments up to but not including design sign off • Allowance for overseeing site activities based on 1x7 hour day shift for pre-condition survey • Associated method statement reviews and general office <p>If your programme changes from the above, could you please provide a programme as early as possible, adopting a worst-case scenario, so that Network Rail resources/costs can be estimated.</p>	
11.	It is unlikely from the scope discussed that you will need to contact Network Rail's Property Dept. to discuss/organise the appropriate wayleave/easement/licence.	
12.	<p>Permanent installations crossing Network Rail property will require an easement/wayleave which must be in place prior to works commencing on site. Temporary/ Permanent occupation of Network Rail land will require a licence. All applications should be made to Network Rail's Property department at PropertyServicesLNEEM@networkrail.co.uk except for statutory suppliers whose works proposed are in relation to utilities and services, in which case please contact the Easements and Wayleave team Easementsandwayleaves@networkrail.co.uk.</p> <p>Please make contact as soon as possible as the process can take a minimum of 12 weeks to put in place.</p>	
13.	<p>Confirmation of level of insurance required:</p> <p>The required level of Public Liability insurance at feasibility stage is: £10,000,000. Network Rail will require sight of the Insurance Certificate ahead of the physical works taking place of £20,000,000.</p>	